

AGENDA

WASHINGTON COUNTY BOARD OF COMMISSIONERS

Agenda Category: Public Hearing – Fourth Reading and Fourth Public Hearing
Land Use & Transportation; County Counsel (CPO 7)

Agenda Title: **CONSIDER PROPOSED A-ENGROSSED ORDINANCE NO. 838 –
AN ORDINANCE AMENDING THE COMPREHENSIVE PLAN
TO IMPLEMENT THE NORTH BETHANY MAIN STREET
URBAN DESIGN PLAN**

Presented by: Andrew Singelakis, Director of Land Use & Transportation
Alan Rappleyea, County Counsel

SUMMARY:

A-Engrossed Ordinance No. 838 proposes to amend the Comprehensive Framework Plan for the Urban Area, the Bethany Community Plan, and the Community Development Code to add requirements applicable to future development within the Main Street area of the North Bethany Subarea. A-Engrossed Ordinance No. 838 is posted on the County's land use ordinance webpage at the following link:

www.co.washington.or.us/landuseordinances

The Board conducted public hearings Aug. 28 and Sept. 18, 2018 for Ordinance No. 838. After its Sept. 18 hearing, the Board directed engrossment of the ordinance to include several amendments. A summary of the amendments were included in the staff report for the Oct. 2 hearing. The Board conducted its first hearing Oct. 2 for A-Engrossed Ordinance No. 838 and continued the hearing to Oct. 23, 2018.

A staff report will be provided to the Board prior to the Oct. 23 hearing and posted on the above land use ordinance webpage. Copies of the report will also be available electronically and at the Clerk’s desk prior to the hearing.

Consistent with Board policy, testimony about the ordinance is limited to two minutes for individuals and five minutes for a representative of a group.

Clerk’s Desk Item: Staff Report *(click to access electronic copy)*

DEPARTMENT’S REQUESTED ACTION:

Read A-Engrossed Ordinance No. 838 by title only and conduct the second public hearing for the engrossed ordinance. At the conclusion of the hearing, adopt A-Engrossed Ordinance No. 838 and associated findings.

COUNTY ADMINISTRATOR’S RECOMMENDATION:

I concur with the requested action.

ADOPTED

Agenda Item No.	<u>3.a.</u>
Date:	10/23/18

BEFORE THE BOARD OF COUNTY COMMISSIONERS Washington County
FOR WASHINGTON COUNTY, OREGON County Clerk

A-ENGROSSED ORDINANCE 838

An Ordinance Amending the Comprehensive Framework Plan for the Urban Area, the Bethany Community Plan, and the Community Development Code Implementing the North Bethany Main Street Urban Design Plan

The Board of County Commissioners of Washington County, Oregon (“Board”) ordains as follows:

SECTION 1

A. The Board recognizes that the Comprehensive Framework Plan for the Urban Area (Volume II) was readopted with amendments on September 9, 1986, and subsequently amended by Ordinance Nos. 343, 382, 432, 459, 471, 480, 483, 516, 517, 526, 551, 555, 561, 571, 572, 588, 590, 598, 608-610, 612-615, 620, 624, 631, 632, 637, 643, 649, 662, 666, 669, 671, 683, 686, 694, 712, 726, 730, 732, 733, 739, 742, 744, 745, 753, 758, 764, 769, 771, 775, 785, 788-790, 796, 799, 802, 805, 809, 813-814, 820, 822, and 828.

B. The Board recognizes that the Bethany Community Plan was adopted by Ordinance Nos. 263 and 265 and subsequently amended by Ordinance Nos. 345, 420, 471, 480, 551, 588, 610, 615, 620, 649, 702, 712, 730, 739, 744-745, 758, 771, 783, 789, 790, 799, 801, and 809.

C. The Board recognizes that the Community Development Code Element of the Comprehensive Plan (Volume IV) was readopted with amendments on September 9, 1986, by way of Ordinance No. 308, and subsequently amended by Ordinance Nos. 321, 326, 336-341, 356-363, 372-378, 380-381, 384-386, 392-393, 397, 399-403, 407, 412-413, 415, 417, 421-

1 423, 428-434, 436-437, 439, 441-443, 449, 451-454, 456-457, 462-464, 467-469, 471, 478-
2 481, 486-489, 504, 506-512, 517-523, 525-526, 528-529, 538, 540, 545, 551-555, 558-561,
3 573, 575-577, 581, 583, 588-589, 591-595, 603-605, 607-610, 612, 615, 617-618, 623-624,
4 628, 631, 634-635, 638, 642, 644-645, 648-649, 654, 659-662, 667, 669, 670, 674, 676-677,
5 682-686, 692, 694-698, 703-704, 708-709, 711-712, 718-720, 722, 725, 730, 732, 735, 739,
6 742-745, 754-758, 760, 762-763, 765-766, 769-776, 782-788, 791-792, 797-802, 804, 809-
7 811, 813-815, 820, 822-824, 826-828, and 831-837, and 840-842.

8 D. The Board recognizes that amendments to several elements of the
9 Comprehensive Plan are necessary to implement the North Bethany Main Street Urban
10 Design Plan. The Plan promotes a Main Street Area design that prioritizes pedestrian and
11 bicycle environments and provides attractive and quality design, both for the public realm
12 (streets and sidewalks) and for private development. These amendments are for the benefit of
13 the health, safety, and general welfare of the residents of Washington County, Oregon.

14 E. Under the provisions of Washington County Charter Chapter X, the
15 Department of Land Use and Transportation has carried out its responsibilities, including
16 preparation of notices, and the County Planning Commission has conducted one or more
17 public hearings on the proposed amendments and has submitted its recommendations to the
18 Board. The Board finds that this Ordinance is based on those recommendations and any
19 modifications made by the Board, as a result of the public hearings process.

20 F. The Board finds and takes public notice that it is in receipt of all matters and
21 information necessary to consider this Ordinance in an adequate manner and finds that this
22 Ordinance complies with the Statewide Planning Goals, the standards for legislative plan

1 adoption as set forth in Chapters 197 and 215 of the Oregon Revised Statutes, the Washington
2 County Charter, the Washington County Community Development Code, and the Washington
3 County Comprehensive Plan.

4 SECTION 2

5 The following exhibits, attached hereto and incorporated herein by reference, are
6 adopted as amendments to the designated documents as follows:

7 A. Exhibit 1 (1 page), amends the Comprehensive Framework Plan for the Urban
8 Area, Policy 43, Map 1 ‘North Bethany Subarea Concept Plan Design Elements.’

9 B. Exhibit 2 (60 pages), amends Chapter 2, the North Bethany Subarea Plan of
10 the Bethany Community Plan:

11 a. Section III. - Background, Planning, Process, Community Design Goals &
12 Plan Amendments;

13 b. Section V. North Bethany Subarea Design Elements;

14 c. Section VI. Neighborhood Design Elements; and

15 d. Section VII. Maps, Drawings, Tables & Guides.

16 C. Exhibit 3 (1 page), amends the Community Development Code:

17 a. Section 390 - North Bethany Subarea Overlay District.

18 SECTION 3

19 All other Comprehensive Plan provisions that have been adopted by prior ordinance,
20 which are not expressly amended or repealed herein, shall remain in full force and effect.

21 ///

22 ///

1 SECTION 4

2 All applications received prior to the effective date shall be processed in accordance
3 with ORS 215.427.

4 SECTION 5

5 If any portion of this Ordinance, including the exhibits, shall for any reason be held
6 invalid or unconstitutional by a body of competent jurisdiction, the remainder shall not be
7 affected thereby and shall remain in full force and effect.

8 SECTION 6

9 The Office of County Counsel and Department of Land Use and Transportation are
10 authorized to prepare planning documents to reflect the changes adopted under Section 2 of
11 this Ordinance, including deleting and adding textual material and maps, renumbering pages
12 or sections, and making any technical changes not affecting the substance of these
13 amendments as necessary to conform to the Washington County Comprehensive Plan format.

14 ///

15 ///

1 SECTION 7

2 This Ordinance shall take effect on November 23, 2018.

3 ENACTED this 23rd day of October, 2018, being the 4th reading
4 and 4th public hearing before the Board of County Commissioners of Washington
5 County, Oregon.

6 BOARD OF COUNTY COMMISSIONERS
7 FOR WASHINGTON COUNTY, OREGON

8 **ADOPTED**

9 *Andy Dard*
CHAIRMAN

10 *A. D. Noyola*
RECORDING SECRETARY

11 READING

PUBLIC HEARING

12 First August 28, 2018
13 Second September 18, 2018
14 Third October 2, 2018
15 Fourth October 23, 2018
16 Fifth _____
17 Sixth _____

12 First August 28, 2018
13 Second September 18, 2018 (Engrossment)
14 Third October 2, 2018
15 Fourth October 23, 2018
16 Fifth _____
17 Sixth _____

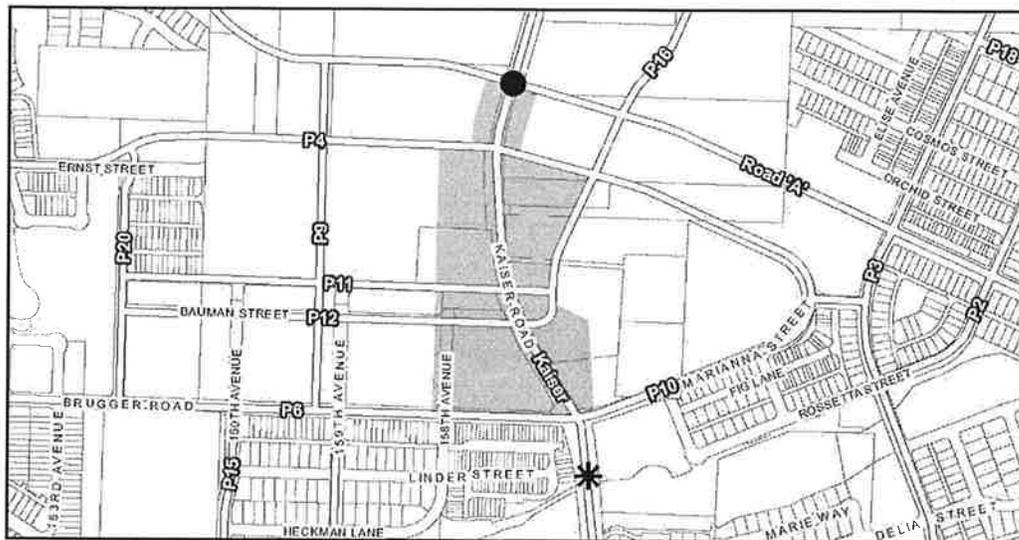
18 VOTE: Aye: Malinowski, Rogers,
Terry, & Duyck

18 Absent:
19 Nay: Schouten

20 Recording Secretary: Ana D. Noyola Date: 10-23-18

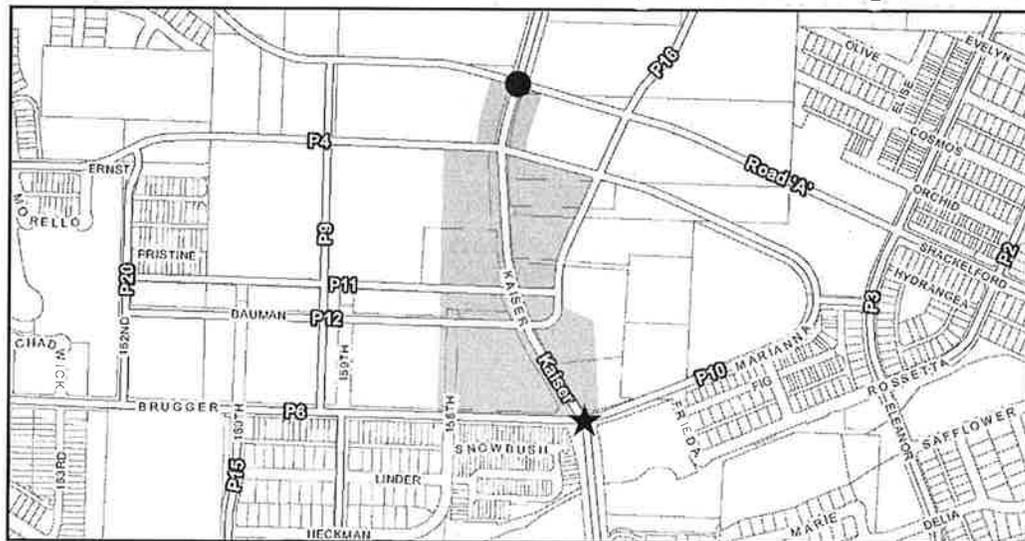
The COMPREHENSIVE FRAMEWORK PLAN FOR THE URBAN AREA, Policy 43, 'North Bethany Subarea Concept Plan Design Elements' Map 1 is amended as shown:

Current Configuration



- * Relocated Community Gateway Location
- Unchanged Community Gateway Location
- Primary Streets (Shown for Context)
- Metro 2040 Main Street Area Designation (Shown for Context)

Proposed Configuration



- ★ Relocated Community Gateway Location
- Unchanged Community Gateway Location
- Primary Streets (Shown for Context)
- Metro 2040 Main Street Area Designation (Shown for Context)

abcdef Proposed additions
abedef Proposed deletions

The North Bethany Subarea Plan of the BETHANY COMMUNITY PLAN is amended as follows:

CHAPTER 2

North Bethany Subarea Plan of the Bethany Community Plan

III. Background, Planning Process, Community Design Goals, & Plan Amendments

C. Planning Process

The North Bethany Subarea Plan is the culmination of a three-year planning process. 601 technical advisory committee and stakeholder work group to develop conceptual and illustrative plans for the UGB expansion area. The planning process established a vision and framework for how new development should occur in the North Bethany area, including land use designations, transportation, stormwater, parks and open space networks, affordable housing and infrastructure funding. An illustrative plan showed how the concept could be configured at the neighborhood level. The North Bethany Subarea Plan implements the land use and transportation related elements of the Concept Plan, as adopted in Policy 43 of the Comprehensive Framework Plan for the Urban Area.

At the core of the North Bethany Subarea is a focal point that is planned for a mix of land uses. The designation of this mixed use area is consistent with the Metro 2040 designation as a Main Street. Section V, Area of Special Concern Road Corridor 2 A North Bethany Main Street Program Guide identifies design ~~standard~~ elements that are specific to development within the designated 2040 Main Street boundary. The North Bethany Funding Strategy and North Bethany Affordable Housing Program implement non-land use aspects of the plans. In addition, local service providers, such as Tualatin Hills Park & Recreation District (THPRD) and Clean Water Services (CWS), have incorporated North Bethany infrastructure and public service requirements into their respective adopted plans.

V. NORTH BETHANY SUBAREA DESIGN ELEMENTS

B. Areas of Special Concern

ASC Road Corridor 2 – North Bethany Main Street, including a central segment of NW Kaiser Road from Bethany Creek (which is north of and parallel to NW Springville Road) to Road A

The requirements of ASC Road Corridor 2 address a central segment of NW Kaiser Road, and the North Bethany Main Street designation on each side of the road. At the time of the adoption of the North Bethany Subarea Plan, a detailed,

~~specific plan was not developed that integrated density, retail design, corner vision, sight distance, building entrances and orientations, access points and the need for turning movements and on-street parking along NW Kaiser Road. Therefore, prior to preliminary engineering of this section of NW Kaiser Road and development of land located within this Area of Special Concern (including portions of lots or parcels), a more detailed Urban Design Plan shall be prepared.~~

~~The County will develop the Urban Design Plan in conjunction with the design for improvements to NW Kaiser Road. As noted in the Main Street Program Guide, an applicant may develop the Urban Design Plan prior to county initiation of the plan by coordinating with the Long Range Planning Section to ensure the plan responds to the North Bethany Main Street Program Guide. The North Bethany Main Street Program Guide contains preliminary commercial programming and design considerations which shall serve as a template for future specific design and development of the area. The commercial program in the Main Street Program Guide is based on the North Bethany Strategic Programming Guide (December 2006) prepared by Leland Consulting Group.~~

1. NW Kaiser Road within the Main Street

~~The segment of NW Kaiser Road, between NW Brugger RoadBethany Creek (north of NW Springville Road) and Road A will bisect North Bethany's Main Street area, which is the portion of the planning area that is planned for the most intensive land uses. While NW Kaiser Road it is designated as an arterial on the Functional Classification Maps in the Transportation System Plan, special design considerations that are not typically associated with arterial streets will apply. These considerations shall include the following:~~

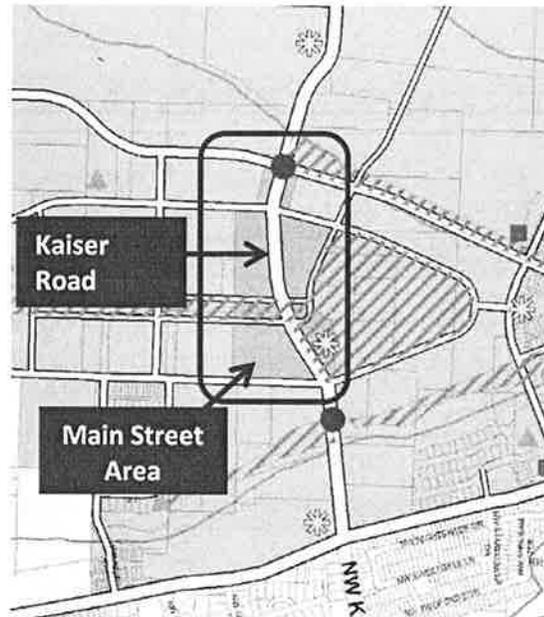
- ~~• Unless review at the land development stage indicates corner vision (CDC Section 418-3) or sight distance issues (CDC Section 501-8.5F), access to NW Kaiser Road from primary streets shown on the Primary Streets Map and nonprimary streets shown on the Neighborhood Plan Maps shall be allowed. Turn restrictions on these allowed accesses may be required as part of the land development review process. Additional accesses shall be consistent with the arterial access spacing requirements of CDC Section 501-8.5.~~
- ~~• For the NW Kaiser Road segmentportion north of NW Brugger Road and south of Road A, street design and on-street parking shall be provided consistent with the Main Street design concept drawing shown in the AR-1b cross-section shown in Section VII.~~
- ~~• Local street connections to NW Kaiser Road may be allowed as direct connections to NW Kaiser Road if determined to be acceptable by the County Engineer.~~

2. North Bethany Main Street

~~The North Bethany Main Street is a planned mixed-use area intended to serve as a focal point of the North Bethany community. The Main Street area is located on both sides of NW Kaiser Road, between NW Brugger Road and Road A, as shown on the North Bethany Subarea Areas of Special Concern Map and illustrated in Figure 1. The Main Street is intended to have a building scale and form that reflects its role as a pedestrian-oriented center, with mixed-use buildings, high~~

density housing, retail uses and services to serve the entire community, and a strong relationship to the adjacent future Park Blocks and East Community Park. The Main Street is also intended to have an attractive streetscape design, with on-street parking and urban design features that create a unique sense of place.

Figure 1 - Location of the Main Street Area



Subsection a. below describes the Urban Design Plan for the Main Street, while Subsection b. describes the design standards that apply to the development of private property and to Kaiser Road street furnishings in the area. Subsection c. provides illustrative cross-sections to show how some of these design elements will relate to the planned design of NW Kaiser Road within the Main Street area.

a. Urban Design Plan for the Main Street

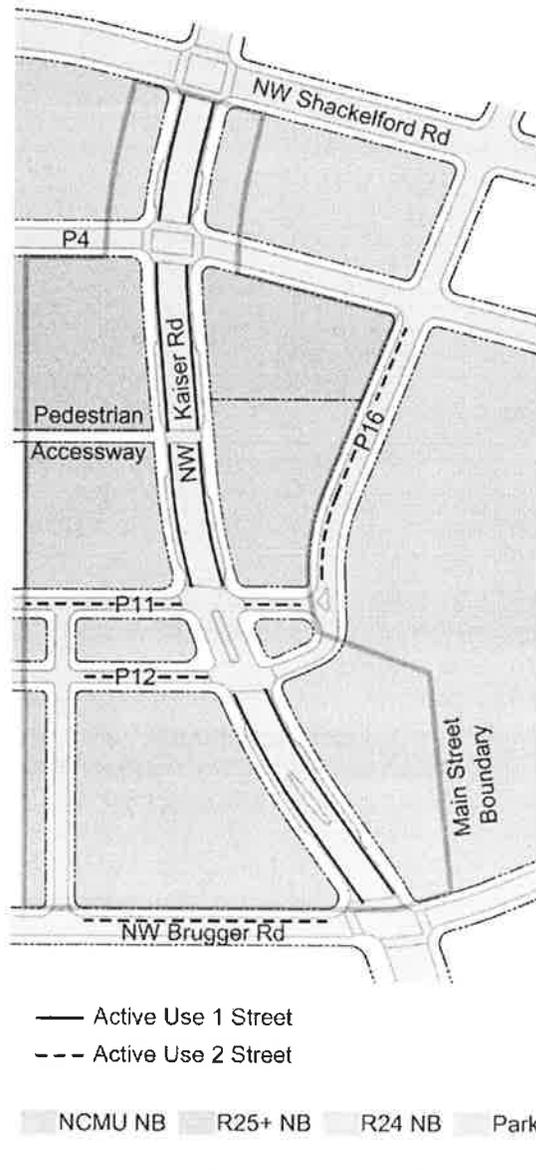
An Urban Design Plan (UDP) has been developed to promote a Main Street design that prioritizes pedestrian and bicycle environments and provides attractive and quality design, both for the public realm (streets and sidewalks) and for private development. Wide sidewalks, bike lanes, and attractive streetscapes will create safe and comfortable spaces for pedestrians and bicyclists. The design of private development should support a pedestrian-friendly environment by providing plenty of interest and activity at the sidewalk. The UDP recommendations have been incorporated into ASC 2 as design standards for future development within the Main Street area.

While pedestrians and bicyclists are the priority in the Main Street area, parking and vehicle driveway access must also be provided. In order to balance pedestrian-friendly design with the needs of vehicles, the design standards are based upon the organizing framework of Active Use Streets. This framework

identifies high-priority walking streets as “Active Use Streets” and targets the most pedestrian-oriented design elements to those streets, while accommodating vehicle access onto development sites from lower-priority streets.

Specific streets within the Main Street area are designated as “Active Use Streets,” as shown in Figure 2. Development along Active Use Streets is subject to additional design standards that are described in more detail in Section b., Main Street Design Standards and Applicability, which is located below.

Figure 2 - Active Use Streets



(1) Active Use 1 Street Elements

abcdef Proposed additions
abcdef Proposed deletions

NW Kaiser Road is the key walking street for North Bethany and is designated as the sole Active Use 1 Street. Development along the Active Use 1 Street frontage (Kaiser Road) is expected to be more “urban” in character than on other streets within the Main Street area, and must be sited and designed to support pedestrian-friendly development. Features include:

- Buildings that:
 - are close to sidewalks
 - occupy most of the street frontage
 - have high transparency (windows and doors)
 - have required articulation
 - have weather protection along sidewalks
- Vehicle parking located behind buildings
- No driveway accesses from NW Kaiser Road into abutting properties

(2) Active Use 2 Street Elements

The Active Use 2 Street designation applies to the block of NW Brugger Road that is within the Main Street, and to segments of Primary Streets P11, P12 and P16 that are adjacent to the Park Blocks and the East Community Park. The Active Use 2 Streets have many of the pedestrian-friendly features of Active Use 1 Streets, but with more relaxed requirements. Features include:

- Buildings that:
 - are close to sidewalks
 - occupy a moderate amount of street frontage
 - have moderate building transparency and weather protection
 - have required articulation
- Vehicle parking located behind or to the side of buildings
- Limited driveway accesses into abutting properties

(3) Non-Designated Streets

The remaining Primary Streets within the Main Street area do not have an Active Use Streets designation, but will still have some applicable design standards such as:

- Required building articulation
- Some required building transparency
- Vehicle driveway accesses into abutting properties are allowed

(4) East-West Pedestrian Accessway

A segment of the east-west pedestrian accessway mapped on the Park, Trails, and Pedestrian Connections Map is an essential pedestrian access point for the Main Street. This accessway segment is located within the Main Street on the west side of Kaiser Road, and runs along the north side of property

designated as Neighborhood Commercial Mixed-Use (NCMU NB). Features of the NCMU NB frontage on the south side of this accessway include:

- Building requirements for:
 - setbacks
 - articulation
 - transparency

The UDP for the Main Street resulted in the development of the design standards described in the following section. These design standards, which apply to land development and Kaiser Road street furnishings in the Main Street area, are intended to promote a Main Street design that prioritizes pedestrian and bicycle environments and provides attractive and quality design for both the public realm (streets and sidewalks) and private development.

b. Main Street Design Standards and Applicability

The Main Street area includes three land use districts, which are depicted in Figure 3. The Neighborhood Commercial Mixed-Use District (NCMU NB) will accommodate a variety of retail and commercial uses, as well as multifamily housing on upper floors; the Residential 25+ NB District (R-25+ NB) will provide areas for multifamily attached housing at a residential density of 20 to 25 units per acre and up to 50 units per acre in certain circumstances; and the Residential 24 NB District (R-24 NB) will accommodate single-family attached and multifamily housing at a residential density of 19 to 24 units per acre. CDC Section 390, North Bethany Subarea Overlay District, provides information about the intent and purpose, permitted uses, and dimensional requirements of these land use districts.

Figure 3 - Land Use Districts in the Main Street Area



The following design standards, described in more detail below, apply to development of properties with an NCMU NB designation, and to Kaiser Road-facing building elevations on properties with R-24 NB and R-25+ NB designations:

- (1) - Setbacks and Frontage Occupancy
- (2) - Building Entrances and Orientation
- (3) - Building Articulation
- (4) - Transparency
- (5) - Weather Protection
- (9) - Street Furnishings

The following design standards, described in more detail below, apply solely to development of properties with an NCMU NB designation and to development at specific intersection corners, regardless of land use district:

- (6) - Building Materials
- (7) - Parking, Loading, and Vehicle Access
- (8) - Emphasized Corners and Main Street Community Gateways

Design Standards:

(1) Setbacks and Frontage Occupancy

Buildings adjacent to the sidewalk provide an engaging experience for pedestrians by allowing passersby to interact with building interiors physically through direct access to entrances, and visually by viewing building interiors through windows and other openings. Minimal building setbacks from the sidewalk help establish the sense of enclosure that creates more comfortable spaces for walking.

Frontage occupancy is the percent of a property's street frontage that is occupied by a building. Minimal setbacks and high frontage occupancy work together to establish a consistent street wall for designated Active Use Streets.

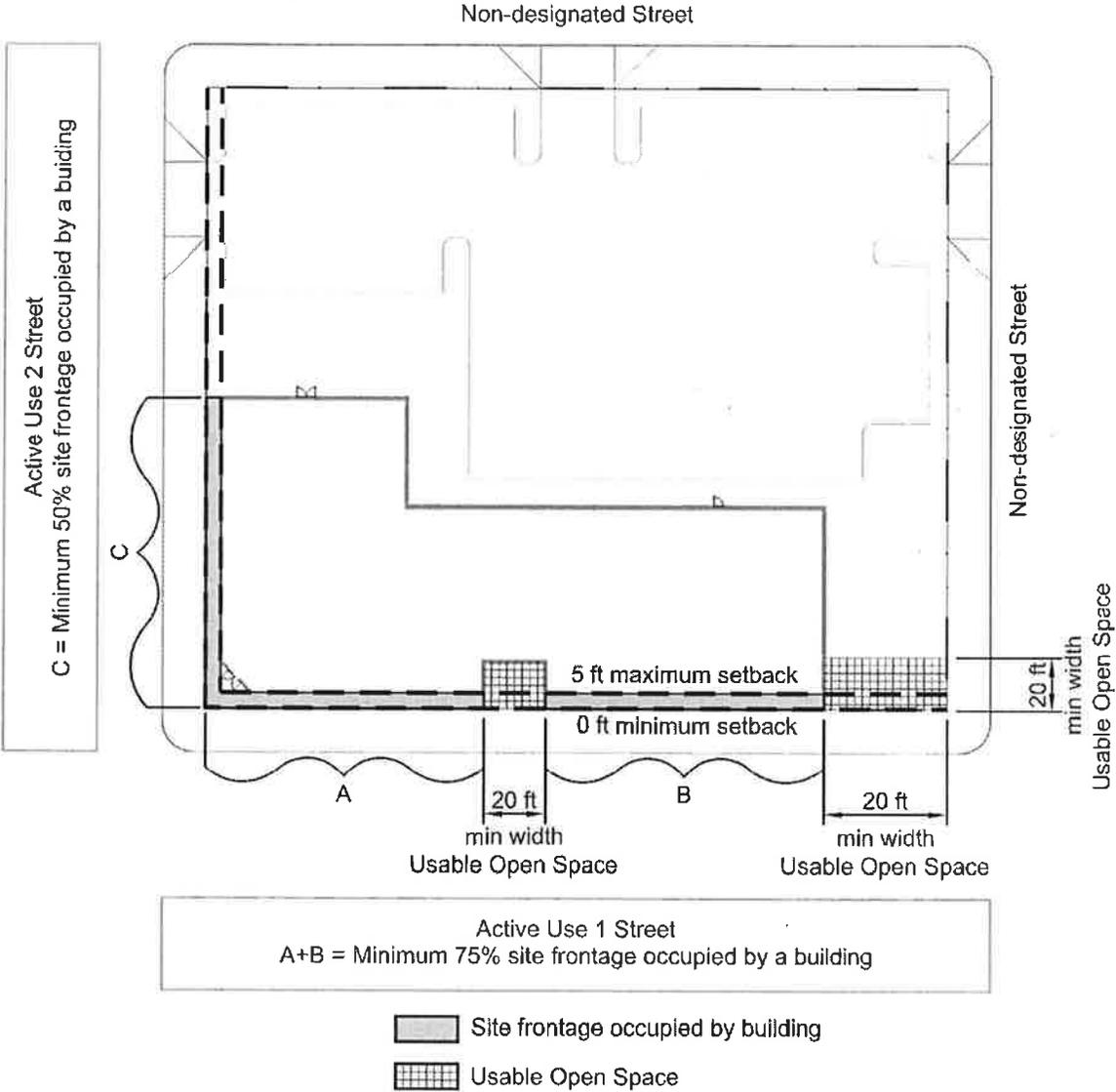
While buildings should be allowed to occupy the full site or block frontage along the street, this may not always be possible or desirable. Therefore, this design element includes an allowance for building recesses, notched corners, and spaces between buildings. Along Kaiser Road, the spaces where buildings do not occupy street frontages should be usable spaces that function as extensions of the sidewalk, providing places for people to meet, rest, and socialize.

Setback and frontage occupancy standards are shown in the table below. Figures 4 and 5 provide illustrative examples of the standards.

Setback and Frontage Occupancy Standards

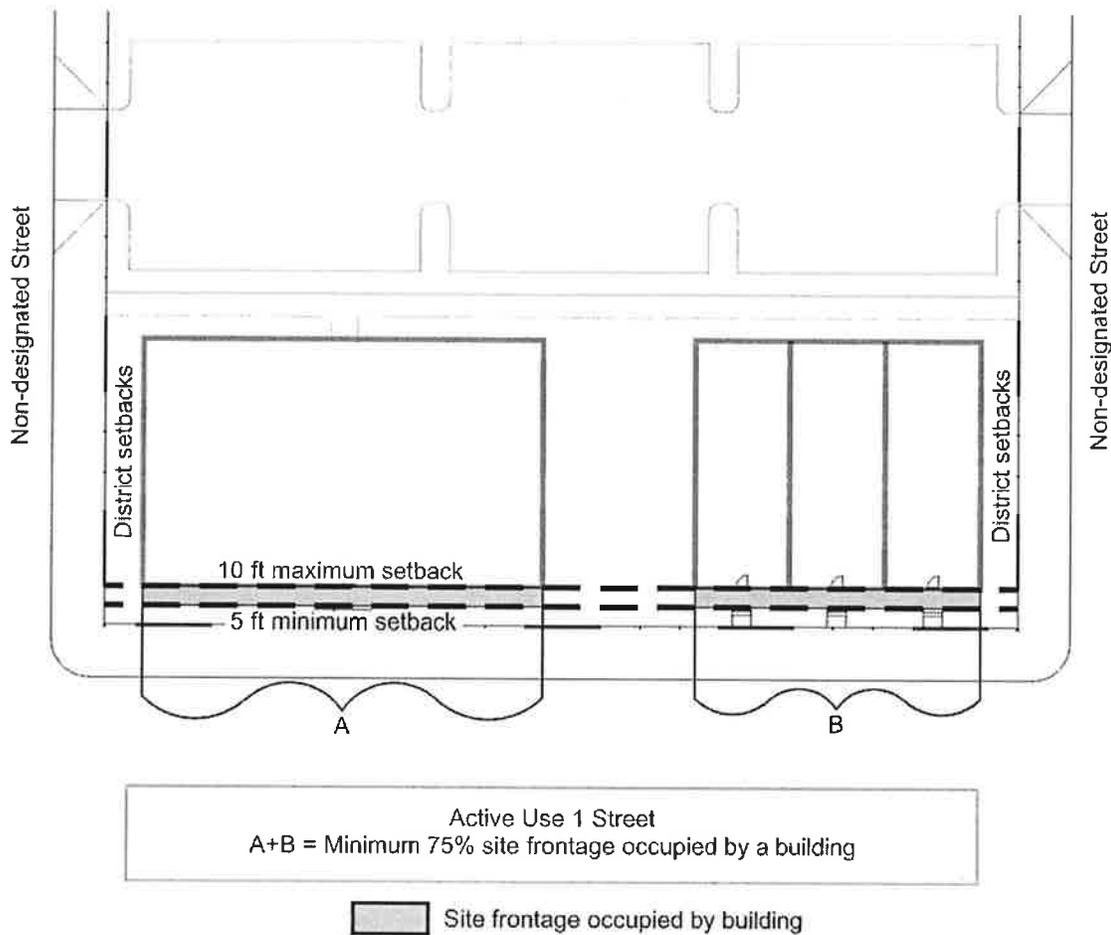
<u>DESIGNATION</u>	<u>NCMU NB</u>	<u>R-24 NB & R-25+ NB</u>
<u>Active Use 1 Street Frontages (Kaiser Road)</u>	<p>Setback: Min. = 0 feet, Max. = 5 feet</p> <ul style="list-style-type: none"> Where finished grade between a building entrance and the adjacent right-of-way exceeds 5%, the maximum setback can be increased to a maximum of 10 feet, in order to provide space for ramps or stairs. <p>At least 75% of a site's street frontage must be occupied by a building located within the minimum and maximum setback range.</p> <p>Where the site's street frontage is not occupied by a building meeting the maximum setback (up to 25% of site frontage allowed), usable open space must be provided.</p> <p><u>Usable open space:</u></p> <ul style="list-style-type: none"> Must have a minimum depth of 20 feet from the lot line and a minimum width of 20 feet. If the building is set back less than 20 feet from the street lot line, usable open space can have a depth of less than 20 feet, but must be provided between the maximum setback and the building façade. Must include three of the following five pedestrian amenities: bench(es), table(s) and chair(s), seat wall(s), fountain(s) or public art. May include pocket parks, pedestrian walkways, landscaping, or stormwater planters as long as pedestrian amenities are also provided. May be partially or entirely paved. 	<p>Setback: Min. = 5 feet, Max. = 10 feet</p> <p>At least 75% of a site frontage must be occupied by a building that meets the maximum setback.</p> <p>Requirements for usable open space are the same as for NCMU NB.</p>
<u>Active Use 2 Street Frontages</u>	<p>Setback: Min. = 0 feet, Max. = 5 feet</p> <p>At least 50% of a site's street frontage must be occupied by a building located within the minimum and maximum setback range.</p>	Not Applicable
<u>Non-designated Street Frontages</u>	<p>Setback: Min. = 0 feet, No Maximum</p> <p>No minimum street frontage occupancy requirement.</p>	Not Applicable
<u>East-West Pedestrian Accessway Frontage</u>	<p>Minimum setbacks shall be scaled according to the height of the NCMU NB building located south of the accessway:</p> <ul style="list-style-type: none"> 1 to 2 stories: 5 feet 3 to 6 stories: 10 feet No maximum setback Setbacks are measured from the building wall to the property line parallel to the accessway. 	Not Applicable

**Figure 4 - Setbacks and Frontage Occupancy Example in the NCMU NB District
 (Plan View)**



abcdef Proposed additions
~~abcdef~~ Proposed deletions

Figure 5 - Setbacks and Frontage Occupancy Example in the R-24 NB and R-25+ NB Districts (Plan View)



(2) Building Entrances and Orientation

Orienting buildings and entrances to the street helps promote an active and engaging street frontage. Building entries are important in making buildings accessible and interesting for pedestrians, and help break down the scale of the building. Buildings in the Main Street area should have entrances oriented to Active Use Streets, with a priority on Active Use 1 (Kaiser Road) street frontages to reinforce walkability on that key walking street.

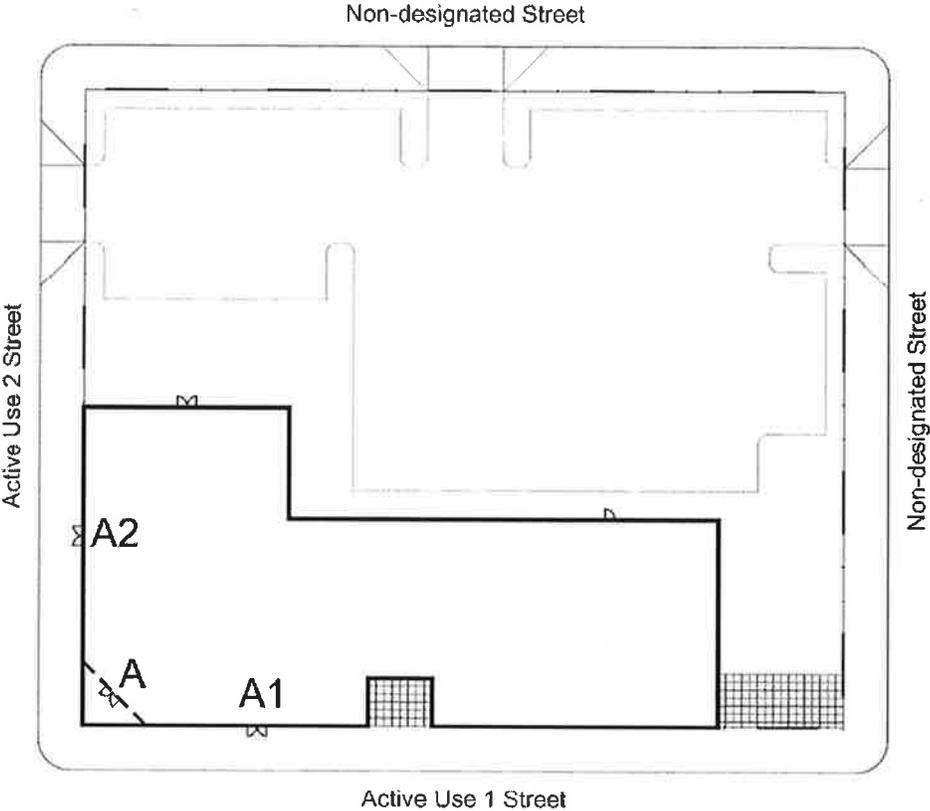
Building entrance and orientation standards are shown in the table below. Figures 6 and 7 provide illustrative examples of the standards.

Building Entrance and Orientation Standards

<u>DESIGNATION</u>	<u>NCMU NB</u>	<u>R-24 NB & R-25+ NB</u>
<u>Active Use 1 & Active Use 2 Street Frontages</u>	<p><u>Buildings with frontage on a designated Active Use Street shall have at least one prominent main entrance oriented to the Active Use Street; the allowed location includes a building corner.</u></p> <p><u>Buildings with frontage on both the Active Use 1 Street (Kaiser Road) and an Active Use 2 Street shall orient the main entrance either to the corner or to the Active Use 1 Street. If the latter, a secondary entrance is required on the Active Use 2 Street.</u></p> <p><u>Main entrances shall be designed with prominent features that distinguish them from other building entrances.</u></p> <ul style="list-style-type: none"> <u>• The main entrance shall be the widest entrance of those provided for use by pedestrians and shall be protected from the weather by canopies or recessed behind the front building façade at least 3 feet.</u> <u>• The main entrance shall be highlighted with at least one of the following architectural features:</u> <ul style="list-style-type: none"> <u>○ Transom windows</u> <u>○ At least two ornamental light fixtures flanking the entry</u> <u>○ Pilasters or columns that frame the doorway.</u> 	<p><u>Buildings with frontage on the Active Use 1 Street (Kaiser Road) shall have at least one prominent primary entrance oriented to the Active Use 1 Street; the allowed location includes the building corner.</u></p> <p><u>The building entrance and orientation standards do not apply to building elevations that face Active Use 2 Street Frontages.</u></p>
<u>Non-designated Street Frontages</u>	<u>No specific entrance or orientation requirements</u>	<u>Not Applicable</u>
<u>East-West Pedestrian Accessway Frontage</u>	<u>No specific entrance or orientation requirements</u>	<u>Not Applicable</u>

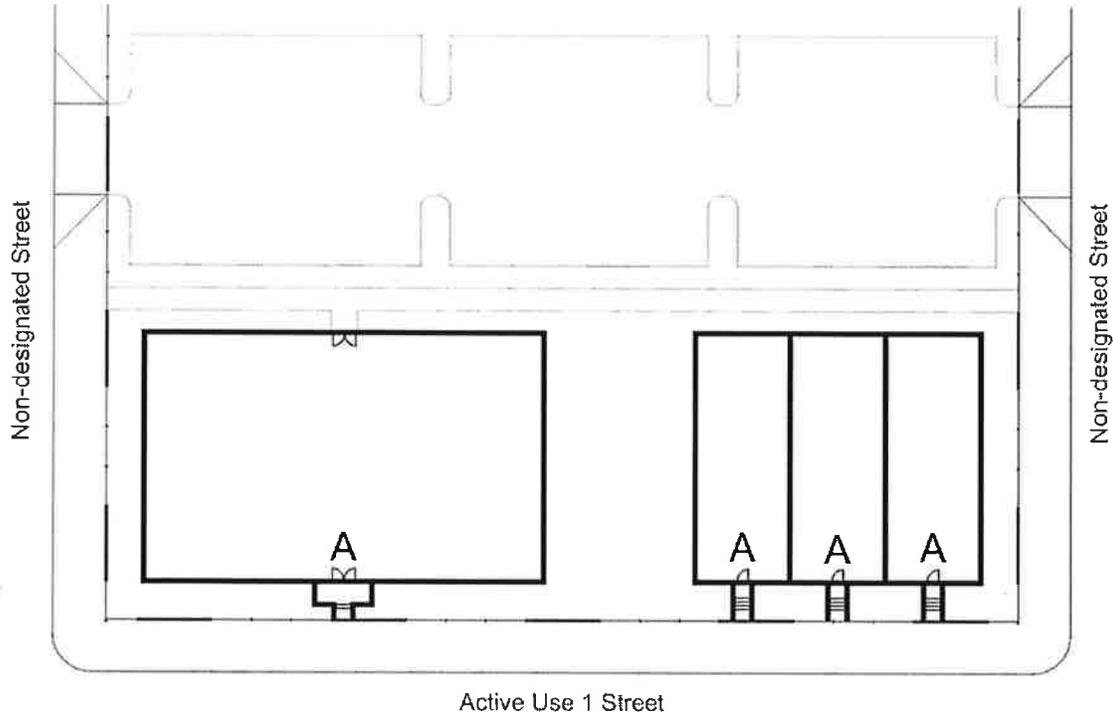
abcdef Proposed additions
~~abcdef~~ Proposed deletions

**Figure 6 - Building Entrance and Orientation Example in the NCMU NB District
(Plan View)**



- A** Provide primary entry at corner
- or
- Provide both:
- A1** Primary Entrance and
- A2** Secondary Entrance

Figure 7 - Building Entrance and Orientation Example in the R-24 NB and R-25+ NB Districts (Plan View)



A Primary entry oriented to Active Use 1 Street

(3) Building Articulation

Articulation describes variation in architectural features that breaks up larger building fronts into smaller planes and masses. Articulation is key to creating visual interest, establishing a rhythm for pedestrians, and maintaining a human scale. Features that create articulation include windows, balconies, recesses, projections, roofline offsets, canopies, or changes in building material. A high degree of articulation shall be provided along street-facing frontages. Articulation shall be used to break up the overall scale of large buildings, thereby reducing perceived building length while still allowing flexibility for development on large sites.

The building articulation standards listed below apply to all street facing façades in the NCMU NB District, and to the building façades in the R-24 NB and R-25+ NB Districts that face NW Kaiser Road, except as noted. Figures 8 and 9 provide illustrative examples of the standards.

Horizontal Articulation Standards:

- Street-facing building elevations shall be articulated with a minimum 2-foot horizontal change in building plane at least every 30 feet. This horizontal articulation standard must be met for each building story.
 - Horizontal articulation features may, but are not required to, extend the full height of the building.
 - The horizontal change in building plane shall be provided through at least two of the following:
 - Projecting bays at least 6 feet wide
 - Building recesses at least 6 feet wide
 - Upper level balconies (projecting or recessed) at least 6 feet wide
 - Front porch or stoop projections, at least 4 feet wide
 - Recessed building entries at least 6 feet wide
 - Green walls at least 6 feet wide may be provided as one of the two required horizontal articulation treatments, in place of a change in building plane. A green wall, sometimes called a “vegetated wall,” living wall,” or “vertical garden,” is a vertical surface designed and planted to be covered at maturity by plants to a minimum height of 10 feet.

Roofline Articulation Standards:

- Roofline articulation must correspond with the horizontal articulation on street-facing building elevations, with roofline changes aligning with horizontal articulation features on the building wall beneath them.
- Roofline articulation shall be accomplished by providing one of the following roofline or roof form elements at least every 30 feet:
 - Gables, dormers, offsets in ridgeline, stepped parapets, cornice lines, or changes in roofline elevation.
- The distance between roofline articulation elements shall be measured from the center point of the element (for example, gable, dormer, offset) to the center point of the next roofline articulation element, or to the edge of the roof if that is the next closest break in the roofline.

Significant Recess Standards (applies to NCMU NB designated sites only):

- When the length of a building exceeds 200 feet, every 100 feet of building length requires a full building height recess that is at least 20 feet wide by 15 feet deep.

Figure 8 - Example Perspective of Horizontal and Roofline Articulation

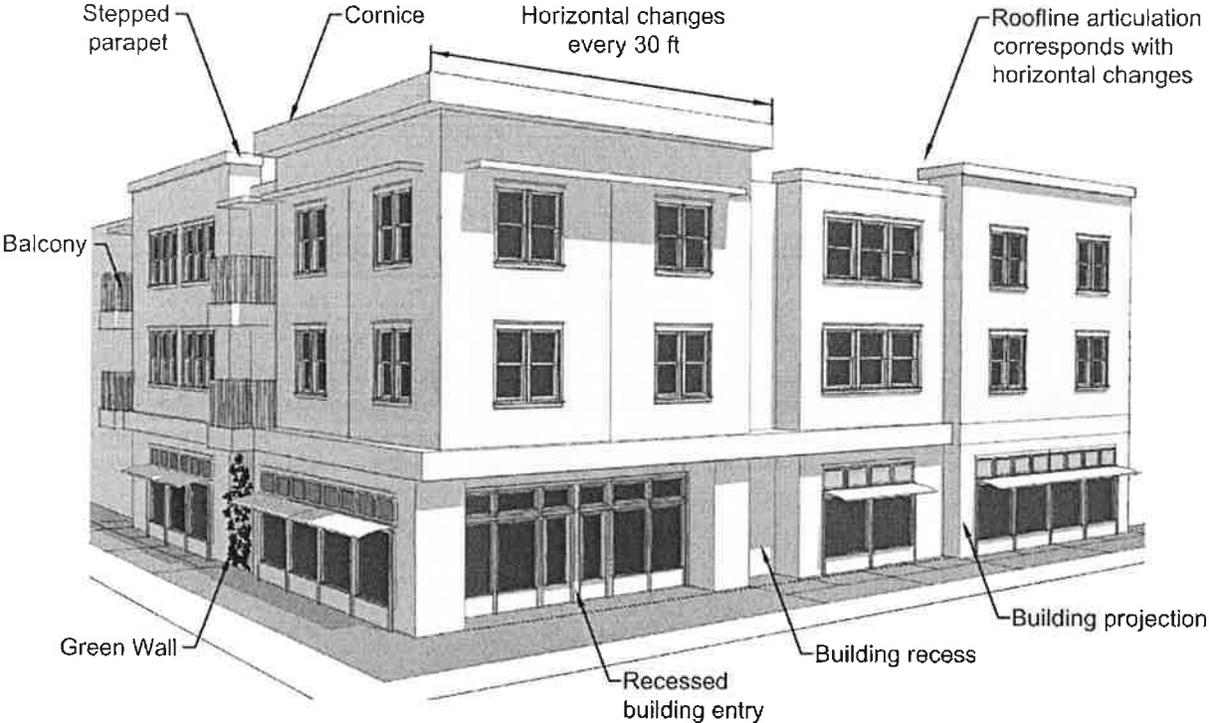
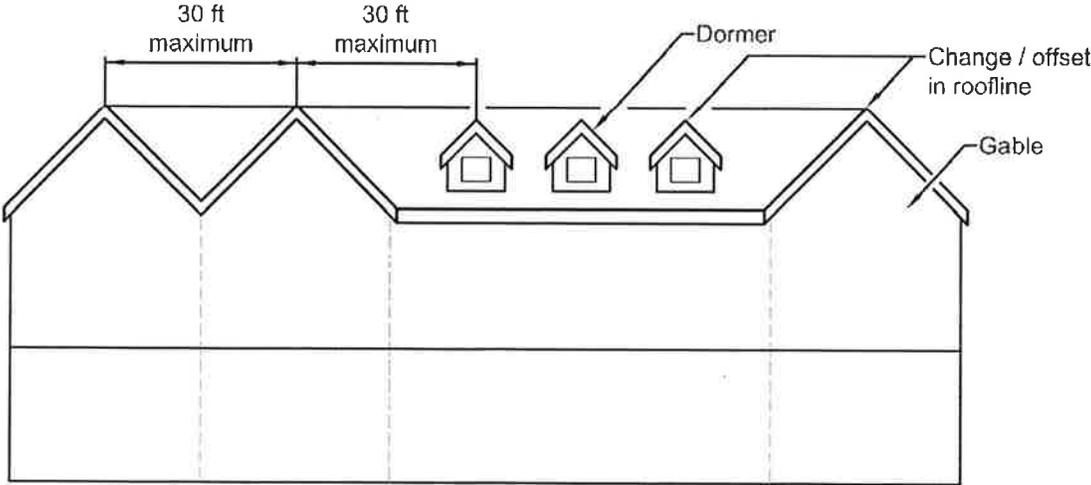


Figure 9 - Roofline Articulation Examples (Elevation)



abcdef Proposed additions
~~abcdef~~ Proposed deletions

(4) Transparency

Transparency refers to the amount of glazing (such as windows and glass doors) on a building façade. High levels of transparency at the ground floor allow pedestrians to see into interior spaces, thereby creating a direct connection between public and private realms and engaging the interest of passersby. Ground floor windows also enhance the safety of public spaces by providing direct visibility to the street. Minimum transparency at upper floors enforces attractive “main street” style building patterns and contributes to building articulation.

High transparency is appropriate for mixed-use areas with active commercial uses at the ground floor, but less so for multifamily buildings where privacy is more of a concern for residents.

Transparency standards are described in the table and bullet list below. Figures 10 and 11 are illustrative examples of building façades having an amount of transparency that is consistent with the standards. The figures are not intended to depict specific required window configurations.

Transparency Standards

<u>DESIGNATION</u>	<u>NCMU NB</u>	<u>R-24 NB & R-25+ NB</u>
<u>Active Use 1 Street Frontages (Kaiser Road)</u>	<u>Ground floor: min. 60%</u> <u>Upper floors: min. 20%</u>	<u>Ground floor: min. 25%</u> <u>Upper floors: min. 20%</u>
<u>Active Use 2 Street Frontages</u>	<u>Ground floor: min. 40%</u> <u>Upper floors: min. 20 %</u>	<u>Not Applicable</u>
<u>Non-designated Street Frontages</u>	<u>Ground floor: min. 30%</u>	<u>Not Applicable</u>
<u>East-West Pedestrian Accessway Frontage</u>	<u>Ground floor: min. 40%</u> <u>Upper floors: min. 20%</u>	<u>Not Applicable</u>

Additional Transparency Standards:

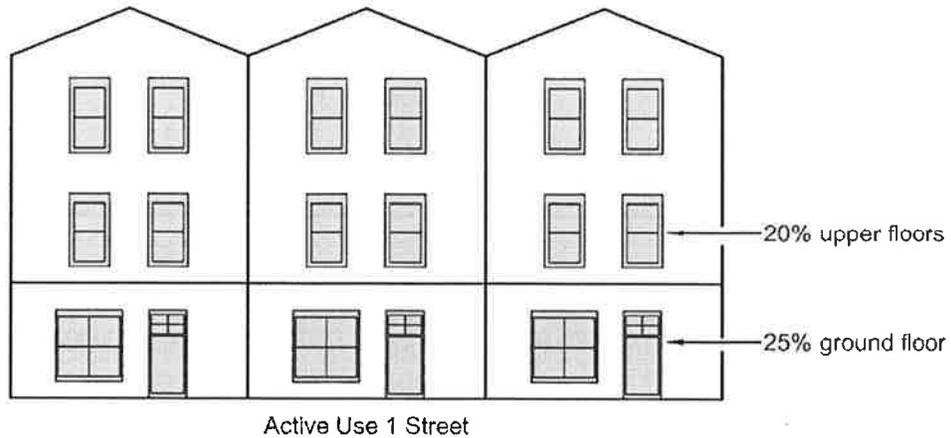
- Transparency is measured as a percentage of wall area.
- Ground floor transparency is measured as the wall area up to the finished ceiling height of the fronting space or 14 feet above finished grade, whichever is less.
- Upper floor transparency is measured as the wall area higher than 14 feet above finished grade.

- All buildings with nonresidential ground floor windows must have a visible transmittance of 0.6 or higher, with the exception of medical and dental offices, which may have tinted windows.
 - Visible transmittance (VT) is a measure of the amount of visible light transmitted through a material (typically glass). Information about visible transmittance typically is, or can be, provided by window manufacturers.
- Minimum transparency is not required on building façades that do not face a street.

Figure 10 - Examples of Building Façade Transparency in the NCMU NB District (Elevation)



Figure 11 - Example of Building Façade Transparency in the R-24 NB and R-25+ NB Districts (Elevation)



(5) Weather Protection

Weather protection features such as awnings, canopies, and recessed entries enhance pedestrian comfort by providing protection against sun and rain. They can also provide visual interest and help define the ground level of buildings, while establishing a human scale for development.

Weather protection standards are described in the table and bullet list below, and Figures 12 and 13 provide illustrative examples of the standards.

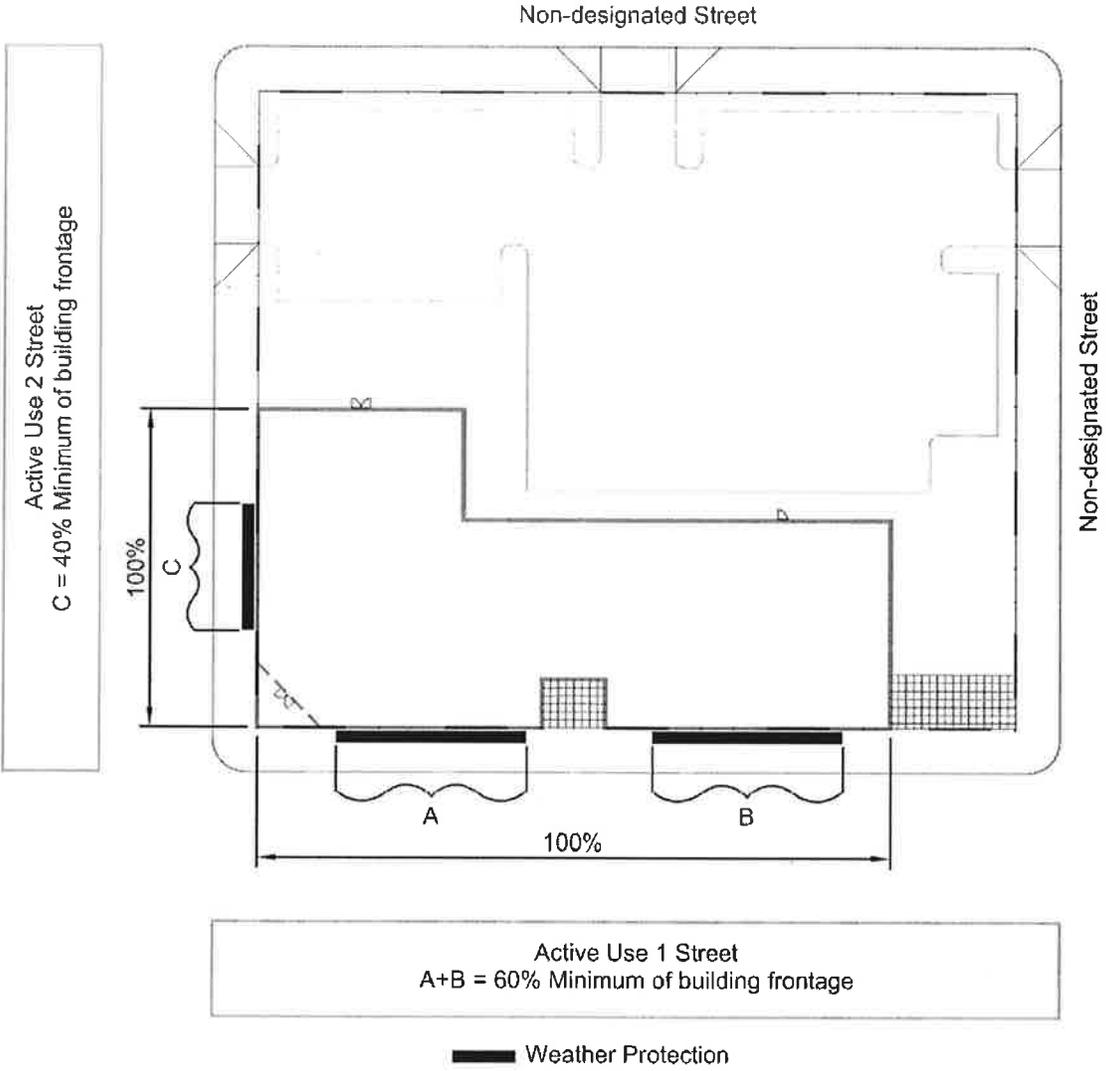
Weather Protection Standards

<u>DESIGNATION</u>	<u>NCMU NB</u>	<u>R-24 NB & R-25+ NB</u>
<u>Active Use 1 Street Frontages (Kaiser Road)</u>	<u>Minimum 60% of building frontage</u>	<u>Protection over all building entries</u>
<u>Active Use 2 Street Frontages</u>	<u>Minimum 40% of building frontage</u>	<u>Not Applicable</u>
<u>Non-designated Street Frontages</u>	<u>Protection over all building entrances open to the public</u>	<u>Not Applicable</u>
<u>East-West Pedestrian Accessway Frontage</u>	<u>None</u>	<u>Not Applicable</u>

Additional Weather Protection Standards:

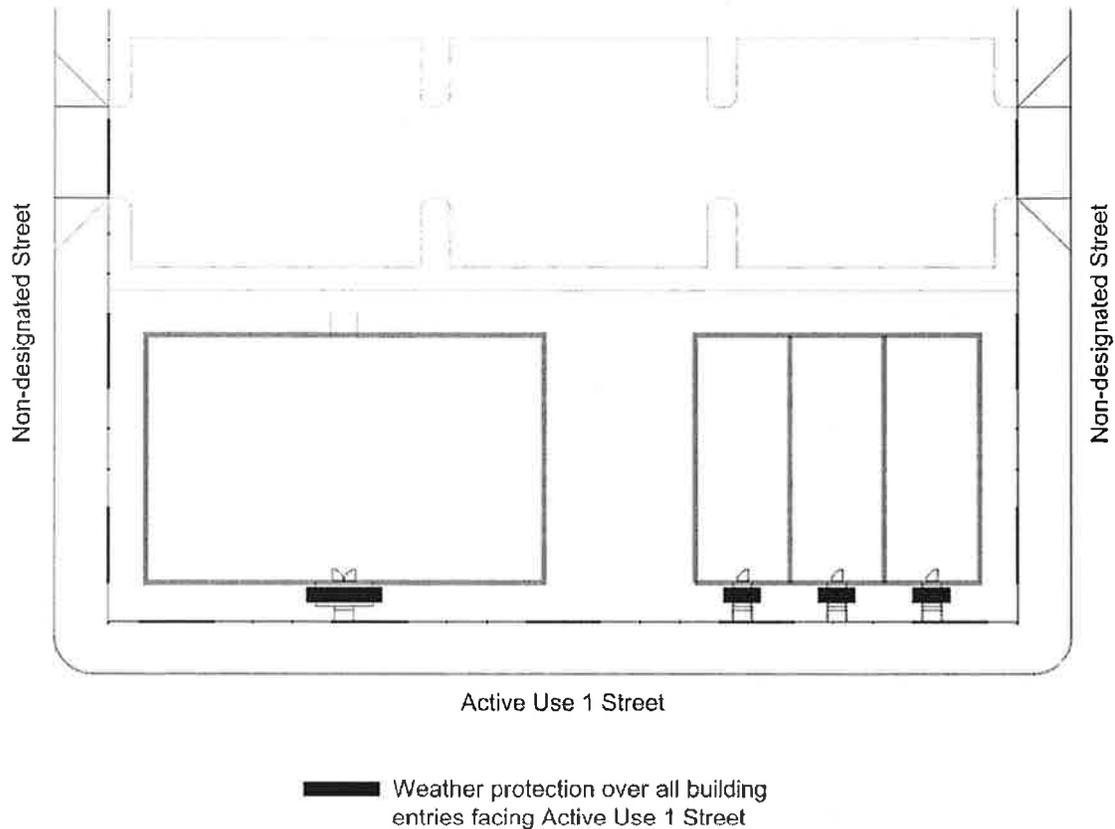
- Weather protection may be provided by awnings, canopies, arcades, colonnades, recessed entries, or a combination of these elements.
- Vertical clearance from the weather protection element to the sidewalk must be between 8 and 12 feet.
- Awnings and canopies must project at least 5 feet from the building façade.
- Awnings and canopies that extend over the right-of-way must be removable. Approval of an Encroachment Agreement (Revocable Permit to Encroach Right-of-Way) must be obtained and recorded for awnings and canopies that extend over the right-of-way.
- Buildings with balconies, structural awnings or canopies, arcades, or colonnades must be set back a sufficient distance to prevent intrusion of these elements into the right-of-way, while still meeting maximum setback and frontage occupancy standards.

Figure 12 - Weather Protection Example in the NCMU NB District (Plan View)



abcdef Proposed additions
abedef Proposed deletions

**Figure 13 - Weather Protection Example in the R-24 NB and R-25+ NB Districts
(Plan View)**



(6) Building Materials

The use of exterior building materials and high-quality finishes can establish a sense of permanence and durability for development in the Main Street area, and using a variety of materials helps provide visual interest. The use of lower-quality building materials, as defined below, shall be prohibited.

The building materials standards listed below apply only to street facing façades in the NCMU NB District.

The following exterior building materials are allowed:

- Masonry (4 inches x 4 inches x 16 inches or smaller)
- Stucco
- Stone
- Wood or cementitious shakes and shingles
- Horizontal lapped siding
- Wood or architectural grade fiber cementitious panel siding
- Concrete-wood mix siding
- Metal (standing seam or panel, painted or galvanized)
- Glass

The following exterior building materials are prohibited:

- T-111 siding, plain or plain painted plywood and strandboard sheets, and similar exterior materials
- Concrete or split-face concrete masonry unit (CMU)

Variety of exterior building materials:

- Each street-facing building façade must include a minimum of two types of exterior building materials, each with an area of at least 20 percent of the façade, except that masonry exterior building material may be used singly and applied to the entirety of the façade

(7) Parking, Loading and Vehicle Access

Vehicle access is often in conflict with pedestrian-friendly design. To promote a safe, comfortable and vibrant pedestrian environment, it is best to limit driveway curb-cuts and surface parking adjacent to sidewalks. Parking and loading should be located behind buildings in the Main Street area to the greatest extent possible.

Landscaping should be used to soften the edges of vehicle areas and provide screening for parking and loading areas. The Community Design Goals in Section III.D. call for integration of stormwater management with other North Bethany Subarea Plan elements. Stormwater planters or low impact development approach (LIDA) facilities can serve as attractive landscaping in addition to their runoff management function, and shall be encouraged to serve this dual function for parking facilities in the Main Street area.

Large surface parking lots should provide safe and comfortable walking paths, to ensure that pedestrians remain a priority. The width and requirements for perimeter parking lot landscaping should provide sufficient visual buffering from the street and to accommodate large shade trees.

The parking, loading and vehicle access standards described in the table and bullet lists below apply only to development in the NCMU NB District. Figure 14 provides an illustrative example of some of the standards.

Parking, Loading and Vehicle Access Standards

<u>DESIGNATION</u>	<u>R-24 NB</u>
<u>Active Use 1 Street Frontages</u> <u>(Kaiser Road)</u>	Vehicle parking is prohibited in the southwest portion of the future East Community Park that is located within the Main Street area.
<u>DESIGNATION</u>	<u>NCMU NB</u>
<u>Active Use 1 Street Frontages</u> <u>(Kaiser Road)</u>	Surface parking and vehicular circulation facilities shall be located behind building(s). For areas where the site's street frontage is not occupied by a building, surface parking and vehicle circulation facilities must be set back a minimum of 20 feet from the lot line abutting the street. Driveway access onto private property is not permitted from Kaiser Road.
<u>Active Use 2 Street Frontages</u>	Surface parking and vehicular circulation facilities shall be located behind buildings, or to the side of building(s), as long as the minimum 50% frontage occupancy standard is met. One driveway access onto a development site is permitted per block.
<u>Non-Designated Street Frontages</u>	No additional standards apply beyond those present in the Community Development Code and other County documents.
<u>East-West Pedestrian Accessway Frontage</u>	Surface parking and vehicular circulation facilities shall be located behind buildings, or to the side of building(s).

Additional Parking, Loading and Vehicle Access Standards:

Parking Lot Perimeter Landscaping

- Where surface parking or vehicular circulation facilities are located adjacent to the right-of-way or east-west pedestrian accessway, perimeter planter strips shall have a minimum 6-foot width, and shall be planted with trees spaced not more than 30 feet on center, and a mix of shrubs and ground cover to provide a landscaped screening buffer.
- LIDA planters or swales may be used to meet some or all of the perimeter landscaping requirement as long as trees and shrubs are also provided.

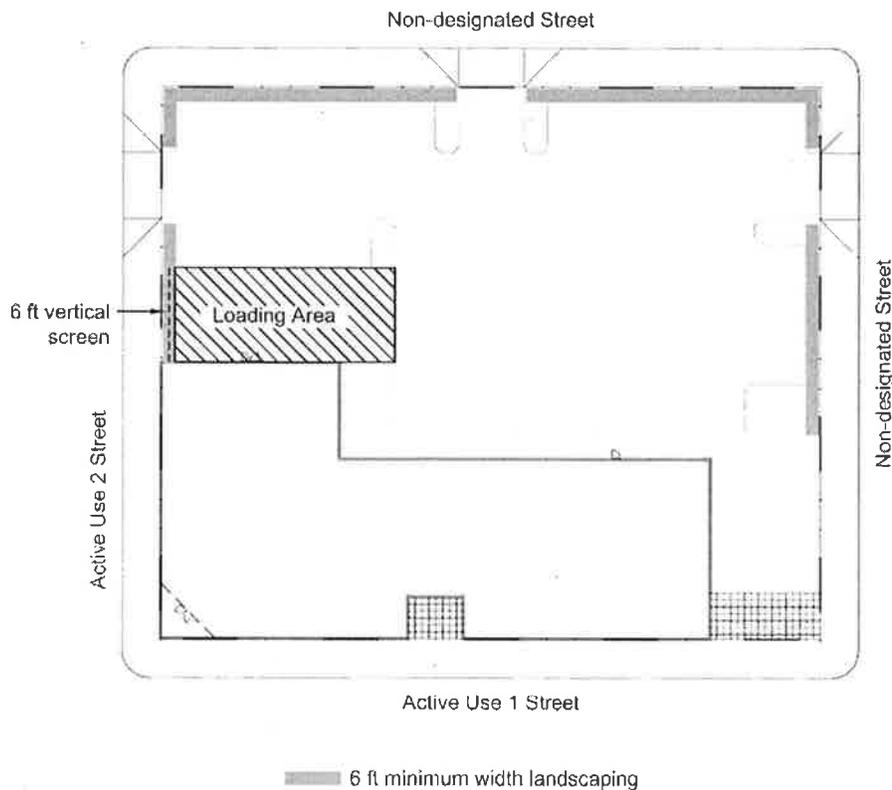
Pedestrian Connections through Parking Lots

- For non-designated street frontages, pedestrian connections through parking lots are required to connect sidewalks along the street to building entrances.
 - Pedestrian connections must be physically separated from adjacent vehicle parking and from parallel vehicle traffic through the use of curbs and landscaping, if not otherwise provided in the parking lot design.
 - Pedestrian connections shall have a minimum 5-foot width and shall be paved or striped to distinguish them from surrounding parking and vehicle circulation areas.

Loading Areas

- For buildings with surface parking, the loading area must be screened or must not be visible from the right-of-way.
- Where loading areas are adjacent to a right-of-way, the required perimeter parking lot landscaping shall include shrubs reaching a height of 6 feet at maturity, or a 6-foot site-obscuring fence, in addition to the required trees.
- Loading shall not interfere with pedestrian circulation.

Figure 14 - Example of Parking, Loading and Vehicle Access in the NCMU NB District (Plan View)



abcdef Proposed additions
abedef Proposed deletions

(8) Emphasized Corners and Main Street Community Gateways

Corner sites will be highly visible within the Main Street area, and are well positioned to activate and add visual interest to the pedestrian realm.

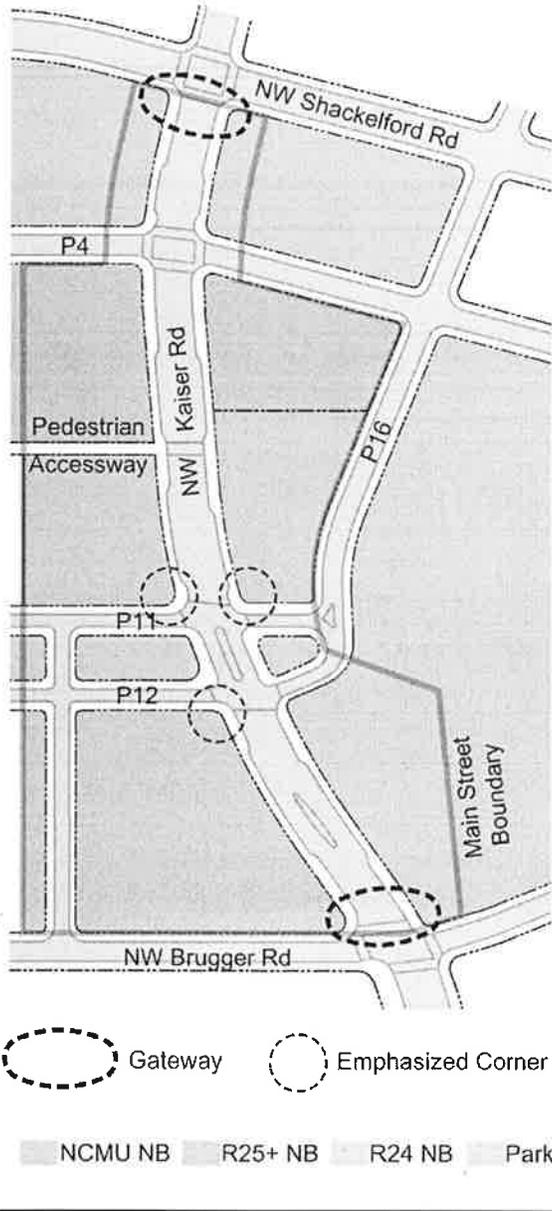
Three corner sites in the NCMU district, depicted in Figure 15, are located at the intersection of two key pedestrian routes, Kaiser Road and the Park Blocks, and frame the heart of the Main Street area. These three corners shall become emphasized corners with prominent design elements. Required design elements shall distinguish the building corners at these sites from the rest of the block with features that create focal points and establish a strong building edge for the street.

Two Community Gateway locations along NW Kaiser Road, depicted in Figure 15, mark the north and south entrances to the Main Street area. These gateways are also identified on the Core Design Elements Map. Gateway features are intended to help define the main entrances to a district, and can create a sense of place by establishing focal points that set the tone for the Main Street's unique character. Development at Community Gateway sites shall be subject to similar standards as Emphasized Corners, but shall feature additional corner design elements to make them even more prominent.

The standards for the Emphasized Corners and Community Gateways are listed below. Figures 16 and 17 provide illustrative examples of the standards.

The Community Gateway standards are tied to the construction of buildings at the gateway corners. The east side of the south gateway is the southwest corner of the East Community Park, and it is possible that a building will not be constructed at that location. However, because of that park corner's visibility and location at the southern entry to the Main Street area, the County believes the corner is critical to the performance and success of the Main Street area. Therefore, during Tualatin Hills Park & Recreation District's (THPRD) future master planning of the East Community Park, the County strongly encourages THPRD to consider siting a plaza space at this corner, with features such as a fountain, public art or a prominent structure to help anchor the corner and serve as a gateway to the community and the Main Street area.

Figure 15 - Location of Emphasized Corners and Community Gateways in the Main Street Area



Standards for Emphasized Corners and Main Street Community Gateways:

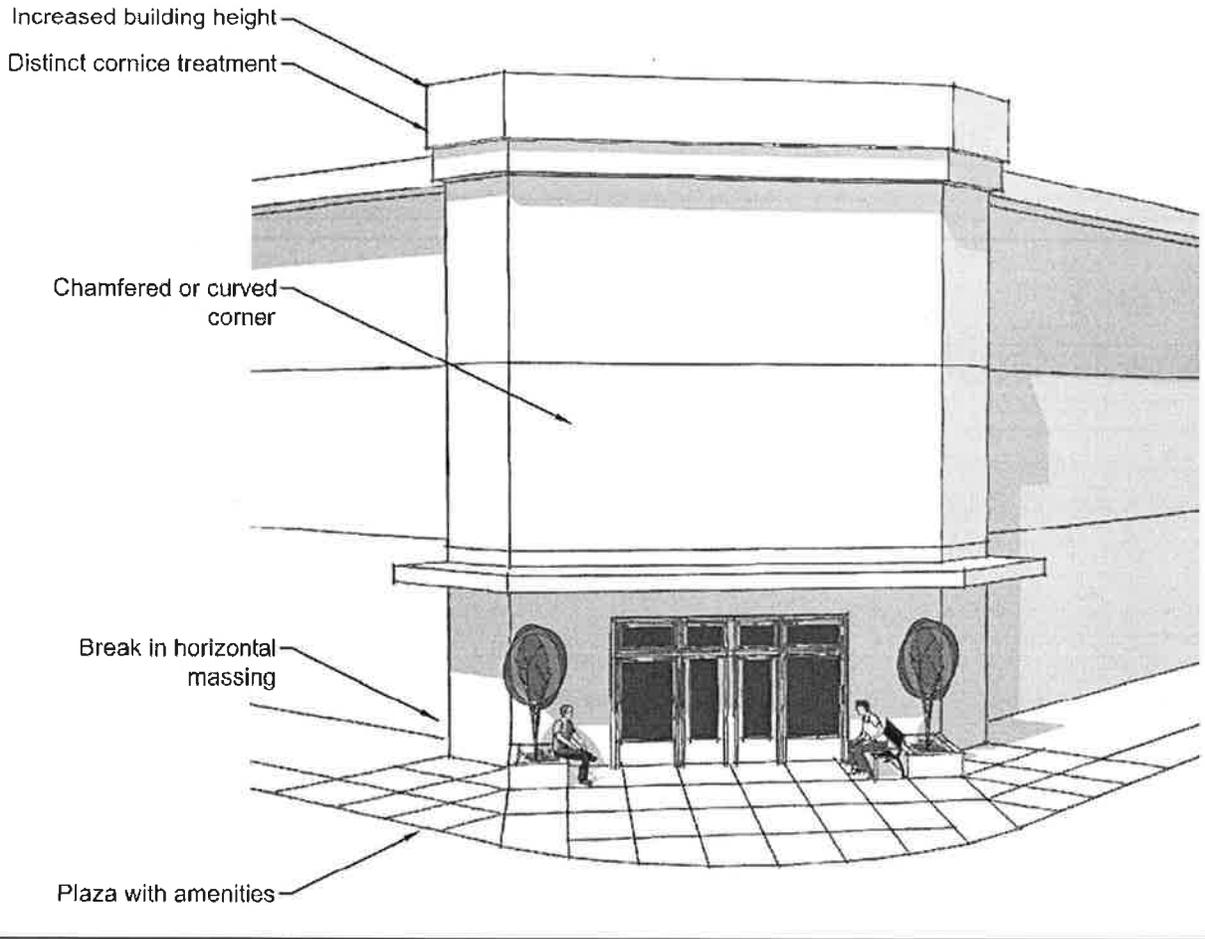
- A. Buildings at the Emphasized Corners identified in Figure 15 shall feature at least two of the treatment options listed in Items 1 - 4 in Section B below. The treatment options listed in Items 1 - 3 shall have a minimum width of 10 feet along both of the intersecting building façades, measured from the building corner to the edge of the treatment option along each façade. (See Figure 17 for an illustration of the 10-foot minimum width measurement.)

abcdef Proposed additions
abcdef Proposed deletions

B. Buildings located at Main Street Community Gateway corners identified in Figure 15 shall feature at least three of the treatment options listed in Items 1 - 5 below. The treatment options listed in Items 1-3 shall be subject to the 10-foot minimum width requirement described in Section A above.

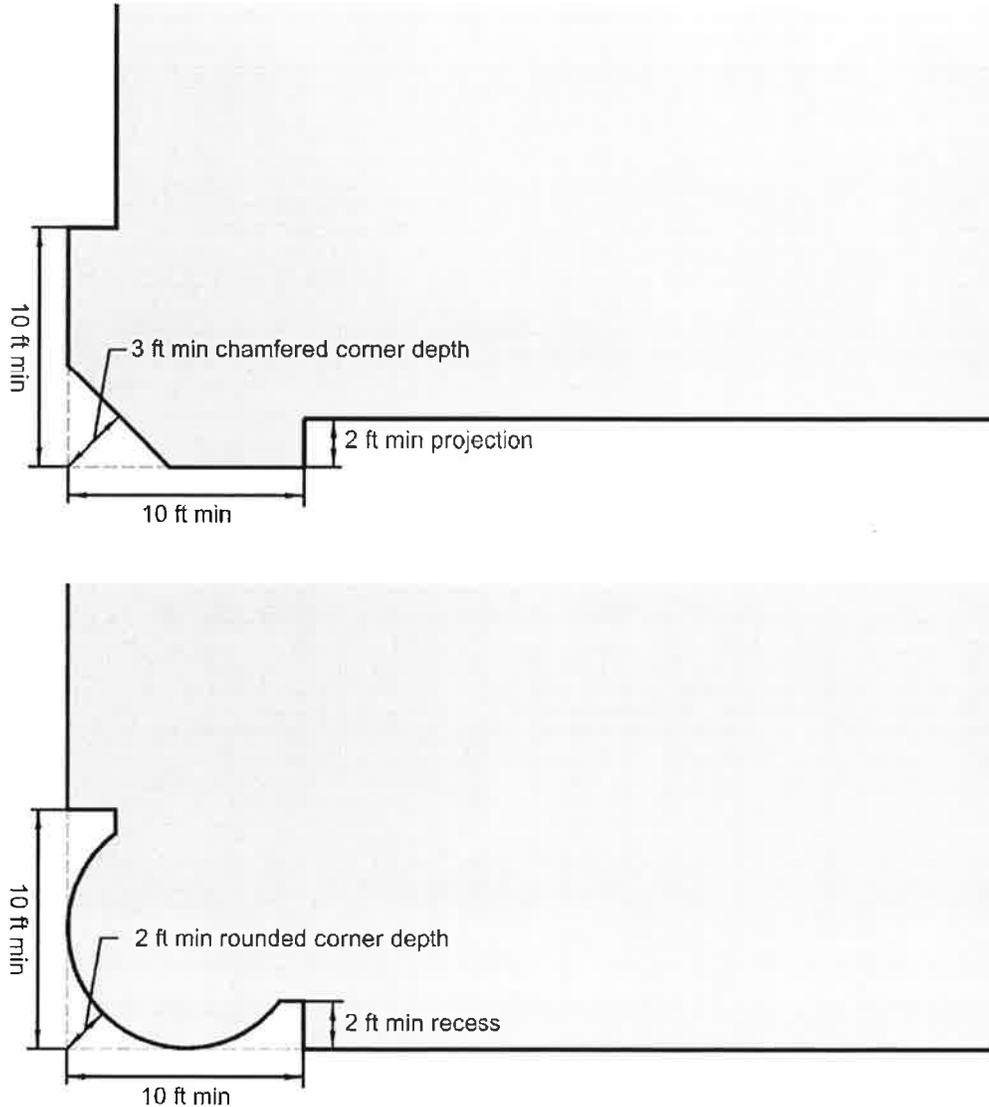
1. Increased building height of at least 10 percent above the height of the remainder of the building.
2. Break in horizontal massing of at least 2 feet; this can be a projection or a recess. If the building has more than one story, the massing break must extend for at least two stories.
3. Distinct cornice treatment that is differentiated from the rest of the building roofline.
4. Chamfered or curved corner. A chamfered corner shall have a 45 degree corner cut and a 3-foot minimum depth. A curved corner shall have a 2-foot minimum rounded corner depth. (See Figure 17 for an illustration of the chamfered corner and curved corner minimum depths.)
5. Plaza space with pedestrian amenities and a distinct landscape or hardscape design that incorporates at least two of the following features:
 - a) Bench(es)
 - b) Seat wall(s)
 - c) Pavers or scored, patterned, textured, or colored concrete
 - d) Landscaping contained in planters or raised planter boxes
 - e) Green wall on an adjacent building. A green wall, sometimes called a "vegetated wall," "living wall," or "vertical garden," is a vertical surface designed and planted to be covered at maturity by plants to a minimum height of 10 feet
 - f) Fountain
 - g) Public art

**Figure 16 - Examples of Emphasized Corner and Community Gateway Treatment Options
(Elevation)**



abcdef Proposed additions
~~abcdef~~ Proposed deletions

Figure 17 - Examples of Chamfered and Curved Corner Treatment Options (Plan View)



(9) Street Furnishings

Some elements of the streetscape design for the Main Street, such as street tree species and streetlight fixtures and poles, were established with the initial adoption of the North Bethany Subarea Plan in 2010 via A-Engrossed Ordinance No. 730. However, the style of street furnishings - benches, trash receptacles and bicycle racks - was not determined.

A distinct palette of street furnishings can help enhance the unique character of a district by distinguishing it from other areas. Therefore, the UDP evaluated styles of street furnishings for the Main Street area.

The street furnishings for the Main Street area should reflect the desired character of North Bethany, a “community of distinction” that promotes high-quality, enduring urban design and integrates natural areas throughout its urban spaces. The street furnishings should also coordinate with the streetlight fixture previously established for use in the Main Street.

Street Furnishings Palette

At the public open houses for the North Bethany Main Street Urban Design Plan, the community chose a preferred street furnishings palette with a minimalist design. This street furnishings palette will be powder-coated black to match the black streetlight fixture that is required along Kaiser Road. Figure 18 shows the bench, trash receptacle and bike rack for this palette.

The street furnishings palette consists of the following products:

Bench:

Product: Austin Bench

Details: Backed with end arms

Manufacturer: Landscapeforms

Mounting: Surface mount

Material: DSTMA wood, Black powder-coat arms and underbody

Contact: 800-430-6206 x1319

Trash Receptacle:

Product: Austin Litter Receptacle

Details: Side opening without lock

Manufacturer: Landscapeforms

Finish: Black powder-coat

Contact: 800-430-6206 x1319

Bike Rack:

Product: Loop Bicycle Rack

Manufacturer: Landscapeforms

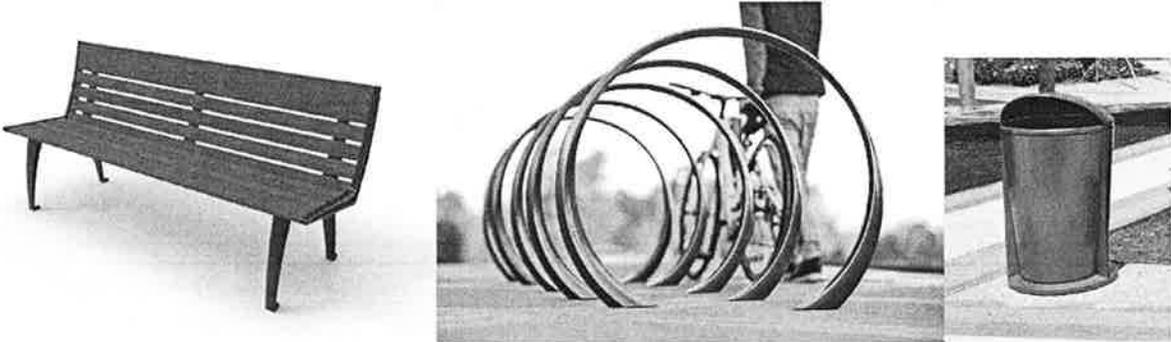
Mounting: Embedded mount

Finish: Black powder-coat

Contact: 800-430-6206 x1319

In the event that the above street furnishings are discontinued or otherwise unavailable, the Planning and Development Services Manager may approve substitute furnishings. The substituted furnishings shall be consistent with the appearance of the approved products.

Figure 18 - Street Furnishings Palette



Street Furnishing Location & Spacing
 Benches, trash receptacles, and bicycle racks shall be located within the street furniture zone between the sidewalk and the curb, and in line with street trees.

Benches, trash receptacles and bicycle racks shall be spaced closer together on Active Use Streets, where there is likely to be the most pedestrian and bicycle activity.

The table below provides the recommended spacing for street furnishings by Active Use Street type. These are recommendations only, recognizing that street furnishing locations may need to be fit around other site features such as street lights and utilities. All street furnishing locations and spacing are subject to County Engineer approval.

Recommended Spacing for Street Furnishings

<u>DESIGNATION</u>	<u>BENCHES</u>	<u>TRASH RECEPTACLES</u>	<u>BIKE RACKS</u>
<u>Active Use 1 Street Frontages (Kaiser Road)</u>	<u>One bench for every 100 feet of building frontage or two per block, whichever is more</u>	<u>One receptacle for every 100 feet of building frontage or two per block, whichever is more</u>	<u>At least one rack for every 100 feet of building frontage or two per block, whichever is more</u>
<u>Active Use 2 Street Frontages</u>	<u>At least one bench per block</u>	<u>At least one receptacle per block</u>	<u>At least one rack per block</u>
<u>Non-Designated Street Frontages</u>	<u>None required</u>	<u>None required</u>	<u>None required</u>

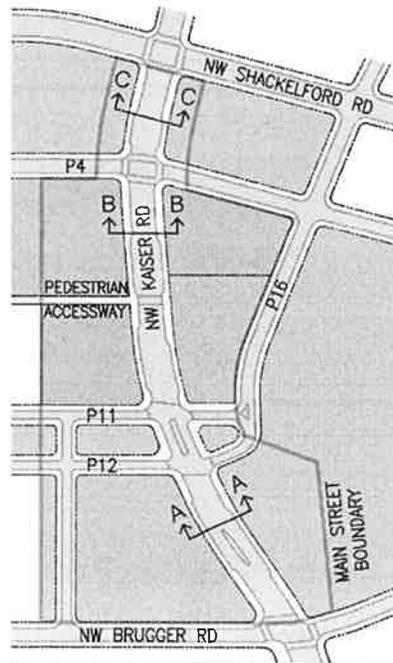
Street Furnishing Maintenance
 Maintenance of street furnishings in the Main Street area is subject to the requirements of Community Development Code (CDC) Section 390-22.5, North Bethany Subarea Overlay District - Road Landscape and Street Furniture Maintenance.

abcdef Proposed additions
 abcdef Proposed deletions

c. Illustrative Street Cross-Sections

The adopted street cross-section for Kaiser Road within the Main Street area, Street Design Type AR-b, includes wide sidewalks, bike lanes, on-street parking, LIDA swales, a planted median, and two vehicle travel lanes. The Kaiser Road right-of-way has a generous width of 102 feet, but street trees and street furnishings will help create a human scale along the street. The following illustrative cross-sections show what Kaiser Road might look like with future development that complies with the ASC 2 design elements, at three points within the Main Street area that are shown in Figure 19.

Figure 19 - Illustrative Cross-Section Locations



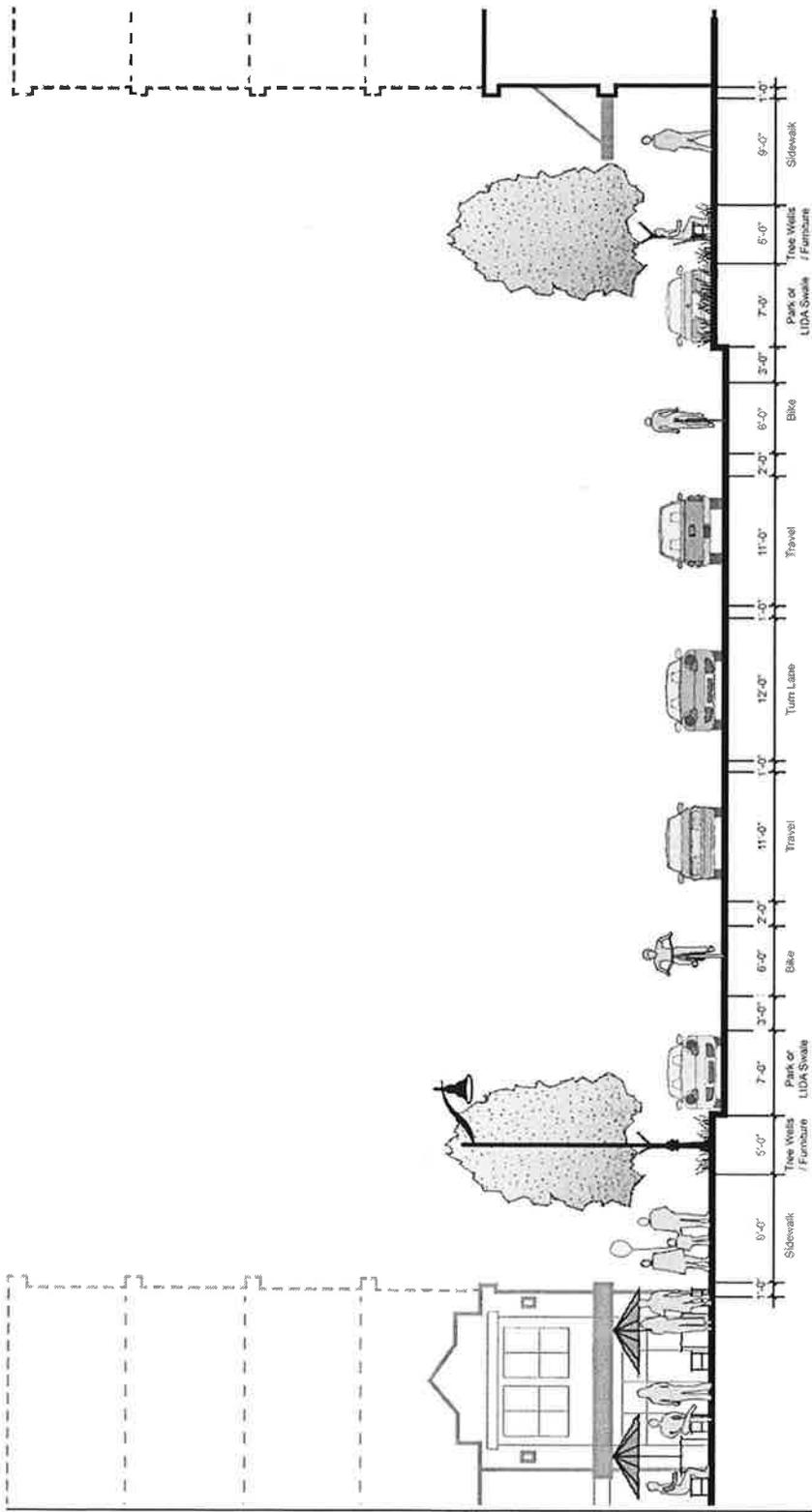
(1) Cross-Section A

Cross-Section A, shown in Figure 20, illustrates NW Kaiser Road at the southern NCMU NB block with the East Community Park to the east. The cross-section illustrates the minimum building height of 20 feet and the maximum height of 65 feet. The cross-section depicts the width of the road right-of-way relative to a future building, thereby illustrating the importance of the building's ability to create a sense of enclosure. The cross-section shows how weather protection such as awnings, street trees and furnishings can help create a more human scale.

(2) Cross-Section B

Cross-Section B, shown in Figure 21, illustrates what the NCMU NB blocks might look like with a pedestrian plaza on one side. While sidewalks on Kaiser Road may provide some space for outdoor seating (such as small café tables), larger seating areas and gathering places should be accommodated in building recesses or spaces between buildings.

Figure 21 – Cross-Section B

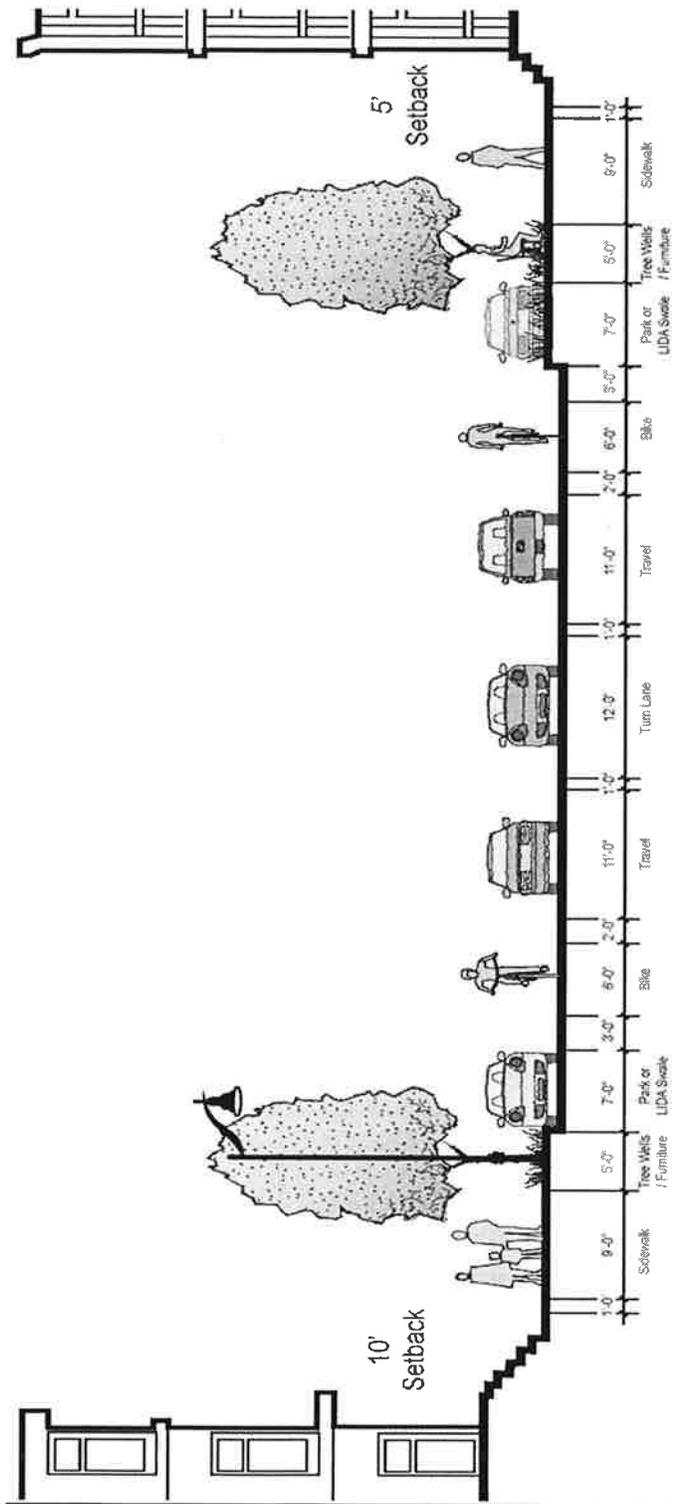


abcdef Proposed additions
~~abcdef~~ Proposed deletions

(3) Cross-Section C

Cross-Section C, shown in Figure 22, illustrates the R-24 NB multifamily blocks on the north end of the Main Street, with minimum and maximum setbacks of 5 and 10 feet, respectively. The Community Development Code has already established a maximum height of 50 feet for this district. The scale of future buildings is likely to be around three stories.

Figure 22 – Cross-Section C



abcdef Proposed additions
 a~~bc~~def Proposed deletions

VI. NEIGHBORHOOD DESIGN ELEMENTS

The North Bethany Subarea is comprised of six neighborhoods, each with a set of distinct Design Elements. These Design Elements are described in text below and illustrated on the Neighborhood Plans. As described under Section IV.C., above, the depicted locations of streets that are not Primary Streets in the Neighborhood Plans are intended as guidance, suggesting one preferred configuration for these transportation improvements.

The design elements of the Areas of Special Concern described above shall apply to development in these areas.

C. Northeast Neighborhood

The Northeast Neighborhood is bordered on the south by Road A. The northern and eastern boundaries are the UGB. On the west, the neighborhood boundary is the north-south powerline corridor and comes to a point where the Abbey Creek tributary and powerline corridor meet the northern UGB.

Design Elements

1. A neighborhood commercial site is adjacent to Primary Streets (P3) and Road A. The site is approximately one-half acre. Building entrances and windows shall be included in these building façades. The site shall include a small public plaza to serve as a community gathering space, as described in ASC 8 (Road A Linear Park).

8. A gateway is identified at the intersection of NW Kaiser Road and Road A on the Core Design Elements Map. The gateway marks the northern entrance to the portion of Kaiser Road designated as a Main Street area on the 2040 Design Types Map (see Policy 40 of the Comprehensive Framework Plan for the Urban Area). Buildings located at the gateway corners on the east and west sides of this intersection shall comply with the design standards for Main Street Community Gateways described in ASC Road Corridor 2. A design plan shall be prepared that addresses the Street Design Map elements, CDC standards and any additional gateway design treatments. A preferred design solution will include the following elements:
 - ~~Vertical element signifying entrance into the commercial district.~~
 - ~~Special detailing on feature elements which may include water, walls, rocks, metal, special paving, traffic calming, enhanced pedestrian crossing and/or seating.~~
 - ~~Minimal or significant amount of landscaping, depending on the gateway design.~~
 - ~~Lighting for pedestrians and to emphasize monument features.~~

Example sketches and photographs illustrating potential vertical and feature elements of this gateway are provided within the Central Neighborhood Design Elements.

D. Southeast Neighborhood

The Southeast Neighborhood boundaries are Road A to the north, the UGB to the east, NW Springville Road to the south and NW Kaiser Road to the west.

The neighborhood borders the Springville Subarea to the south and street and trail connections between the two areas are integral to the design. Bethany Creek flows from east to west and is planned as an expanded stream corridor with a trail and regional stormwater swales along the creek. Prominent features of the built landscape include Bethany Presbyterian Church, a proposed fire station, the proposed civic use center, the East Community Park, and the Main Street Area primarily west of NW Kaiser Road (a portion of which is located on the east side of NW Kaiser Road).

Design Elements

1. The following design elements shall be incorporated into the portion of the neighborhood between Bethany Creek and NW Springville Road:
 - a. The preferred location for the 1.5- to 2-acre neighborhood park is contiguous to Bethany Creek and adjacent to the proposed fire station and the neighborhood commercial site. The proposed fire station and the neighborhood park may be separated by a local street or pedestrian path;
 - b. The Subarea street pattern shall have an east-west street alignment through this area with access to NW Springville Road at spacing approved by the County Engineer;
 - c. A neighborhood commercial site, approximately two-thirds of an acre, is adjacent to NW Springville Road and Primary Street P3; and
 - d. Designated access points shall be provided consistent with the Primary Streets Map. Additional access to NW Springville Road may be provided at spacing approved by the County Engineer.
2. The southwest corner of the East Community Park is the planned location of a civic use (e.g., community center) and shall have a design that connects the civic use to the adjacent community park. The civic use building is designated as a Design Feature and shall be located on the site consistent with the orientation arrows depicted on the Neighborhood Plan Map. Specifically, the building shall be visible from NW Kaiser Road and the Park Blocks. Additional complementary design features include building orientation to front the park and the placement of a plaza between the park and the civic use.
3. The East Community Park shall be approximately 15 acres. The tree groves in the southwestern corner shall be preserved. The park anchors the eastern end of the ridge and views shall extend to the east, terminating at the east end of Primary

Street P4 (as denoted by the Design Feature symbol on the Core Design Elements Map). The front façades of all buildings, including residential and commercial uses, shall front on the park consistent with Design Element 7.

4. Trails and pedestrian connections shown on the Parks, Trails and Pedestrian Connections Map shall be provided that are consistent with THPRD standards, and that include the following elements:
 - ~~a.~~ An east-west pedestrian trail north of the civic center shall connect the eastern community park with the Main Street Area on the west side of NW Kaiser Road.
 - ab. Two pedestrian crossings shall span Bethany Creek in locations that align with street connections across NW Springville Road.
 - bc. Pedestrian and bicycle connections to and across NW Springville Road and NW Kaiser Road are important and shall be developed consistent with the Parks, Trails and Pedestrian Connections Map.
5. The Southeast Neighborhood has eleven Primary Streets. Primary Streets shall be provided in new development consistent with Section IV.C. (Primary Streets) and Section V.B. (Areas of Special Concern) of the North Bethany Subarea Plan. The intent of each Primary Street is provided below:

- g. Road A is a Primary Street and its development shall be consistent with ASC Road Corridor 1A. A gateway is identified, including provision for Gateway design standards at the intersection of Road A and NW Kaiser Road on the Core Design Elements Map. That gateway marks the northern entrance to the designated Main Street area. Buildings located at the gateway corners in the southeast and southwest sides of the intersection shall comply with the design standards for Main Street Community Gateways described in ASC Road Corridor 2.

E. Central Neighborhood

The Central Neighborhood extends north from NW Springville Road to Road A. Generally, the western boundary is the Waterhouse Powerline Trail Corridor and the eastern boundary is NW Kaiser Road.

The most prominent natural features of this neighborhood are the ridge that runs east-west along the highest point, and the associated views of the surrounding area including the Tualatin Mountains, Chehalem Mountains and Tualatin River valley. Other notable features include an ash forest/wetland south of the park blocks and Bethany Creek.

The key design parameters for the Main Street area require the central section of NW Kaiser Road to have the following characteristics:

- A “main street” design (on-street parking, pedestrian scale and urban design features);¹⁷
- A strong relationship to the park blocks and East and West Community Parks;¹⁷
- A building scale and form which reflects its role as the focal point of the community;¹⁷ and
- A civic use in a central and prominent location with an adjacent public gathering area.

~~Preliminary programming and design considerations for the Main Street area are provided in the North Bethany Main Street Program Guide for the North Bethany Subarea, which shall serve as a template for future urban design plan(s) and development of the area.~~

Design Elements

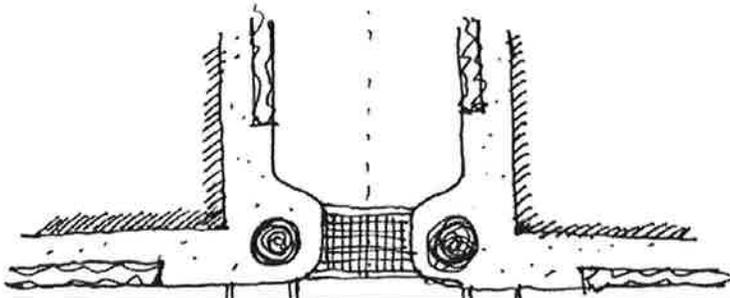
1. The Central Neighborhood shall have two neighborhood parks and the linear park blocks.
 - a. A 1.5- to 2-acre northern neighborhood park shall be south of Road A and adjacent to the Waterhouse Powerline Trail Corridor or the Abbey Creek tributary.
 - b. A 1.5- to 2-acre southern neighborhood park near Bethany Creek will have a connection to Bethany Creek and provide a trailhead for the multi-purpose path north of and parallel to Bethany Creek. The park shall have street frontage along its north side that can accommodate on-street parking for park users. As such, this street shall be designed to be no less than the minimum county standard for parking on two sides. The front façades of residential buildings on this street shall face the park.
 - c. The Park Blocks shall be consistent with ASC 5.
 2. The location of higher density along the central ridge and the Main Street area and the streetscape design along the park blocks are intended to make these primary community amenities immediately accessible to a large number of residents. The park blocks will support future transit through the placement of the highest residential densities along the park blocks and design standards in the Community Development Code.
- ***
5. Buildings constructed as part of the community park shall be placed near and oriented to adjacent streets and used to define the park edge.
 6. ~~The design and development of the Main Street area shall be consistent with the requirements of ASC Road Corridor 2, the intent of the North Bethany Main Street Program Guide. This guide is intended to be consistent with the intent of the Metro 2040 Main Streets designation. Examples include high density housing, mixed use buildings, retail and services that are intended to serve the entire North Bethany Subarea.~~

7. Development shall be consistent with ASC Road Corridors 1B, 2 and 4A and ASC 5, 7, 9 and 10.
8. Gateways are identified at the intersections of NW Kaiser Road/Road A and NW Kaiser Road/NW Brugger Road on the Core Design Elements Map. These gateways mark the northern and southern entrances to the designated Main Street (see Policy 40 of the Comprehensive Framework Plan for the Urban Area). Buildings located at the gateway corners on the east and west sides of these intersections shall comply with the design standards for Main Street Community Gateways described in ASC Road Corridor 2. The intersections of NW Kaiser Road/Road A and NW Kaiser Road with the east-west trail on the north side of Bethany Creek are gateways that mark the north and south entrances to the portion of NW Kaiser Road designated as a Main Street on the 2040 Design Types Map (see Policy 40 of the Comprehensive Framework Plan for the Urban Area). The gateways are identified on the Core Design Elements Map. A design plan shall be prepared that addresses the Neighborhood Plan and Street Design Map elements, applicable CDC standards and any additional gateway design treatments. A preferred design solution for each gateway will include the following elements:
- ~~• Vertical element signifying entrance into the commercial district.~~
 - ~~• Special detailing on the feature elements, which may include water, walls, rocks, metal, special paving, traffic calming, enhanced pedestrian crossing and/or seating.~~
 - ~~• Minimal or significant amount of landscaping, depending on the gateway design.~~
 - ~~• Lighting for pedestrians and to emphasize monument features.~~

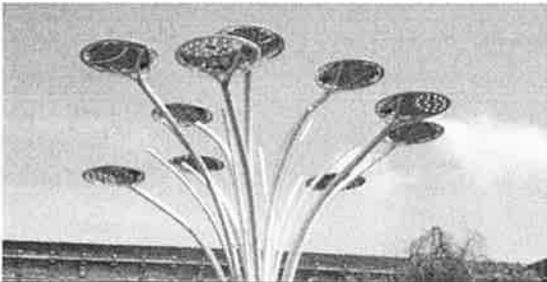
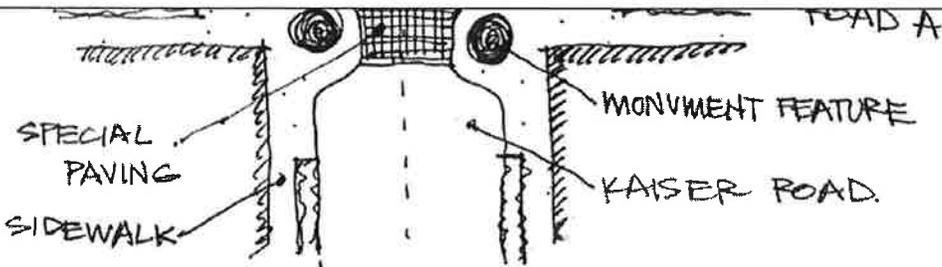
~~The gateways at the intersection of NW Kaiser Road and the east-west trail on the north side of Bethany Creek may be located in proximity to the trail on each side of NW Kaiser Road, or may be shifted north to the intersection of NW Kaiser Road and NW Brugger Road.~~

~~The following sketches and photographs illustrate examples of potential vertical and feature elements for this gateway.~~

CORNER LOCATION



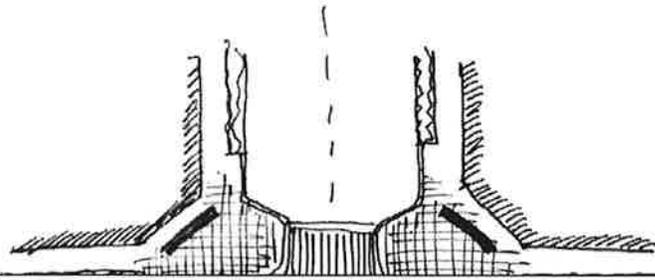
DELETED



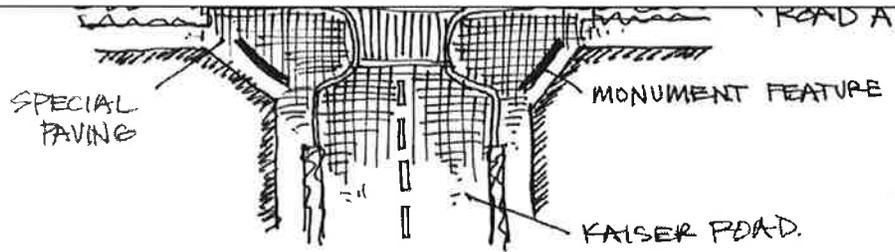
DELETED



SPECIAL PAVING



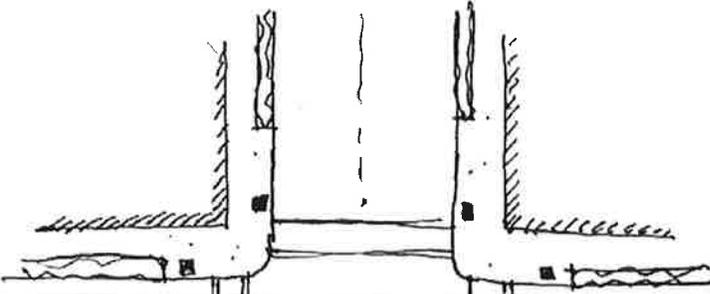
DELETED



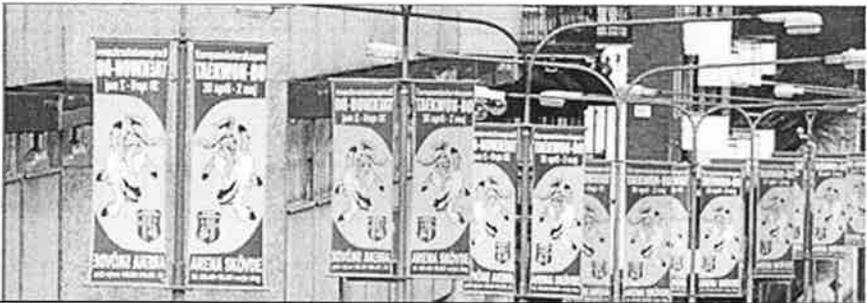
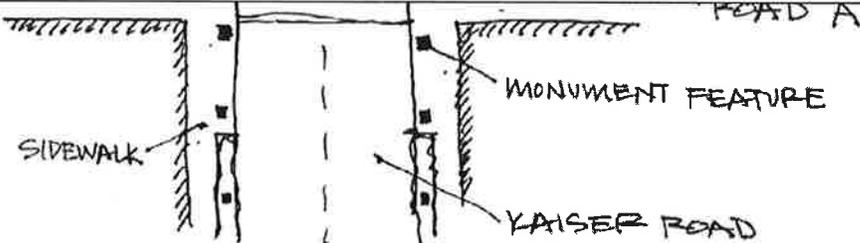
DELETED



REPEATED VERTICAL ELEMENTS



DELETED



DELETED

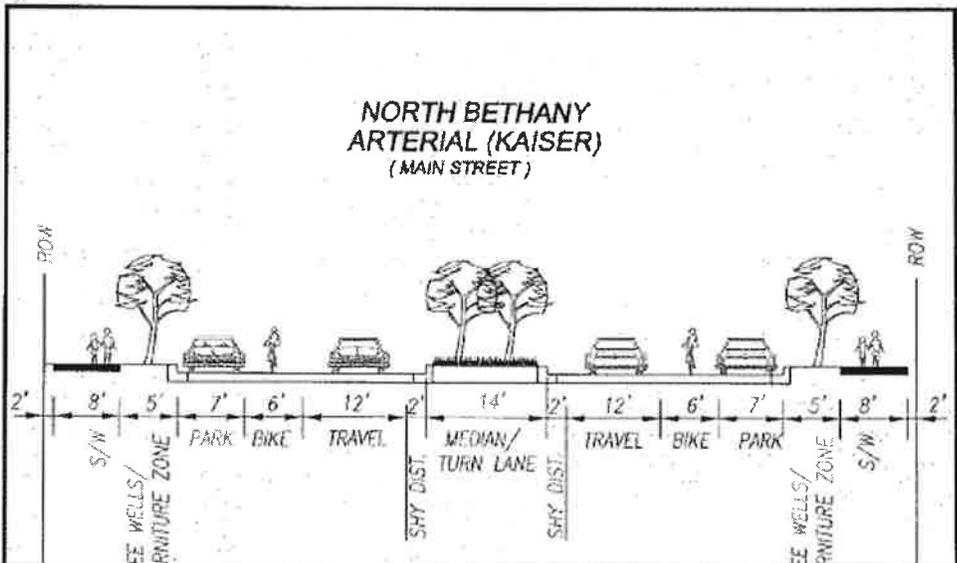


abcdef Proposed additions
abcdef Proposed deletions

VII. MAPS, DRAWINGS, TABLES & GUIDES

Subsection A – Street Design Cross Sections

Repeal the following cross-section Street Design Type AR-b and replace with updated cross-section Street Design Type AR-b.



R E P E A L E D

CRITERIA

VEHICLE LANE WIDTHS:	12 FT.
ON STREET PARKING:	7 FT.
BICYCLE LANES:	6 FT.
SIDEWALKS:	8 FT.
TREE WELLS/ FURNITURE ZONE	5 FT.
MEDIANS/TURN LANE WIDTHS:	12 FT. - 16 FT.
NEIGHBORHOOD TRAFFIC MANAGEMENT:	NOT APPROPRIATE

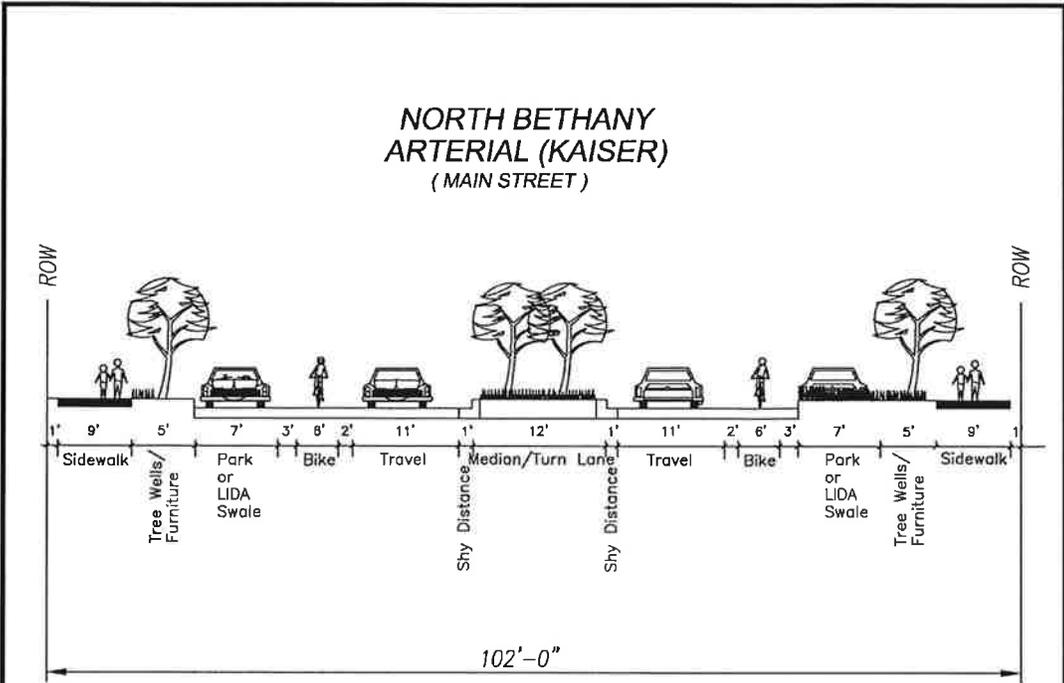
NOTE: 20 FT. CLEAR BETWEEN PARKING AND RAISED MEDIAN FOR EMERGENCY RESPONSE REQUIRED.

94-11-01100074(V)RES2018 DWG. PAC. NORTH BETHANY, TAP. N1000. PLLOT STAMP: 06.

 DEPARTMENT OF LAND USE AND TRANSPORTATION	PROJECT NUMBER	NORTH BETHANY CONCEPTS STREET CROSS SECTIONS WASHINGTON COUNTY, OREGON ARTERIAL (KAISER) (MAIN STREET)
	100074	

abcdef Proposed additions
 abedef Proposed deletions

Street Design Type AR-b



PATH: H:\PROJECT10_CAD\ROB\ CAD: NBETHANY.DWG, TAG: NBKB PLOT STAMP: 06/04/18 8:46A KATHERINES

CRITERIA

VEHICLE LANE WIDTHS:	11 FT.
ON STREET PARKING:	7 FT.
BICYCLE LANES:	6 FT.
SIDEWALKS:	9 FT.
TREE WELLS/ FURNITURE ZONE	5 FT.
MEDIANS/TURN LANE WIDTHS:	12 FT. - 16 FT.
NEIGHBORHOOD TRAFFIC MANAGEMENT:	NOT APPROPRIATE

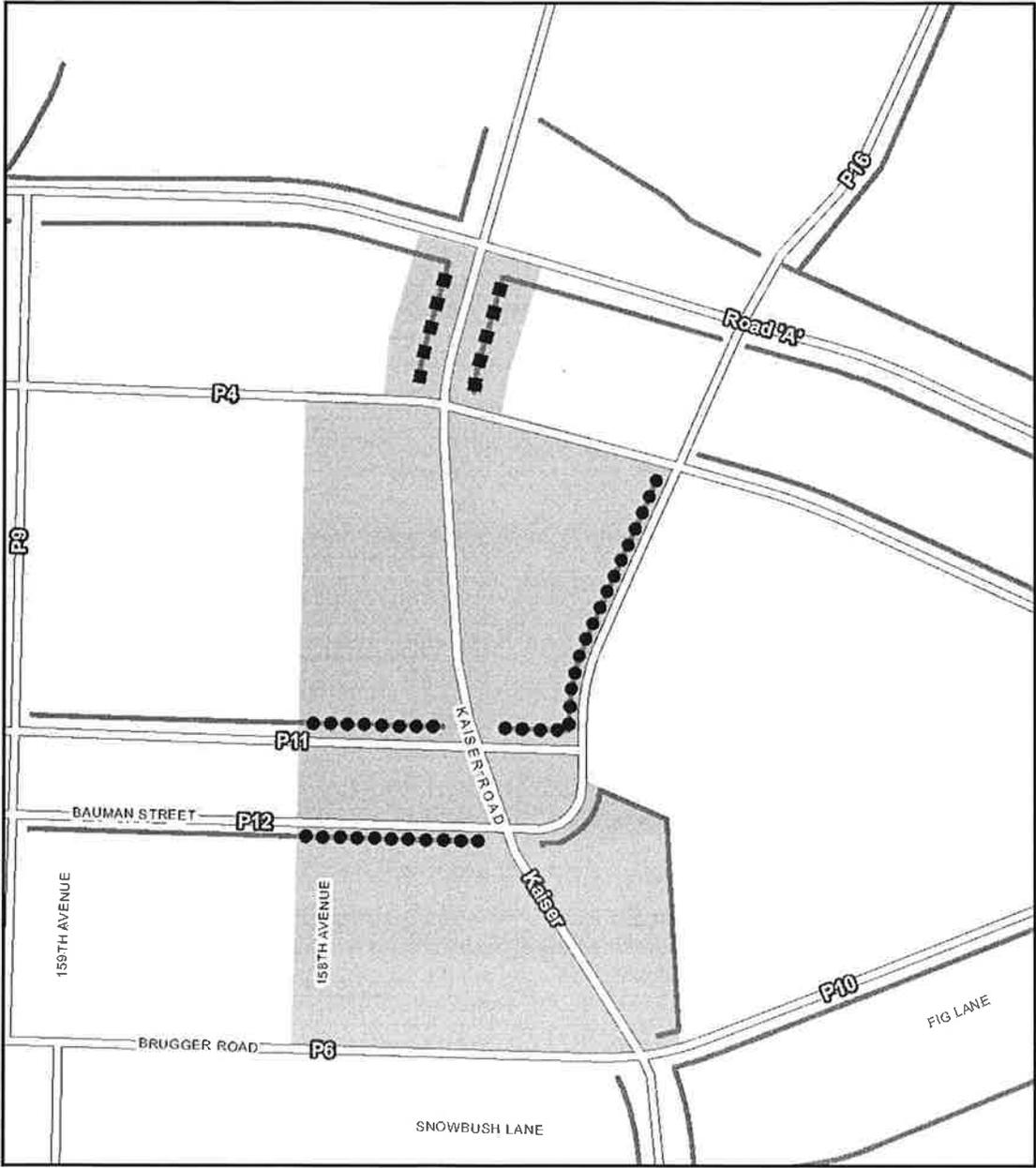
NOTE: 20 FT. CLEAR BETWEEN PARKING AND RAISED MEDIAN FOR EMERGENCY RESPONSE REQUIRED.

DEPARTMENT OF LAND USE AND TRANSPORTATION	PROJECT NUMBER	NORTH BETHANY CONCEPTS STREET CROSS SECTIONS WASHINGTON COUNTY, OREGON
	100074	ARTERIAL (KAISER) (MAIN STREET)

abcdef Proposed additions
 abcdef Proposed deletions

Subsection C – Community Plan Maps

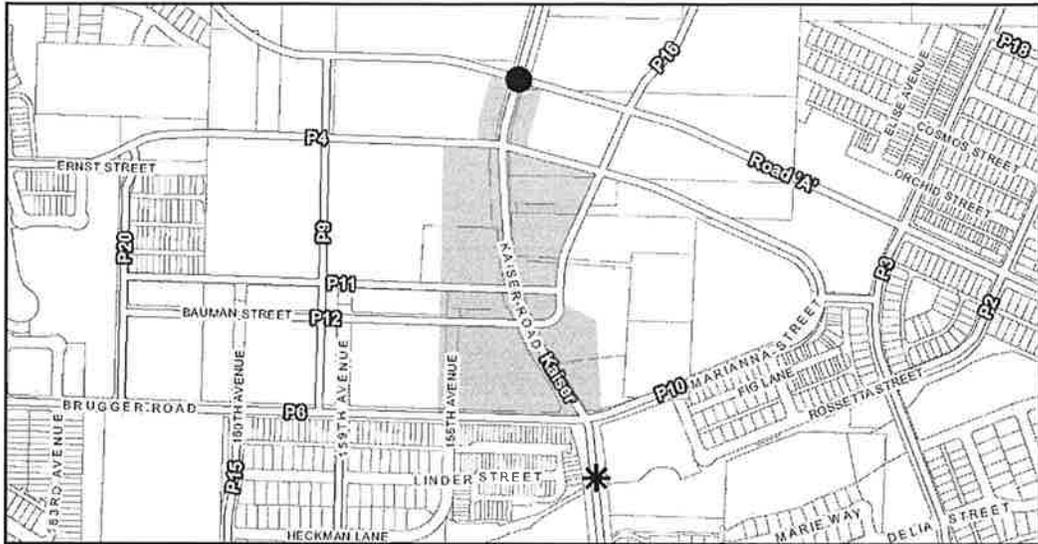
The 'Special Frontages' map is amended as shown:



- Remove Category A Special Frontage
- ■ ■ Remove Category B Special Frontage
- Unchanged Special Frontages (Shown for Context)
- Primary Streets (Shown for Context)
- Metro 2040 Main Street Area Designation (Shown for Context)

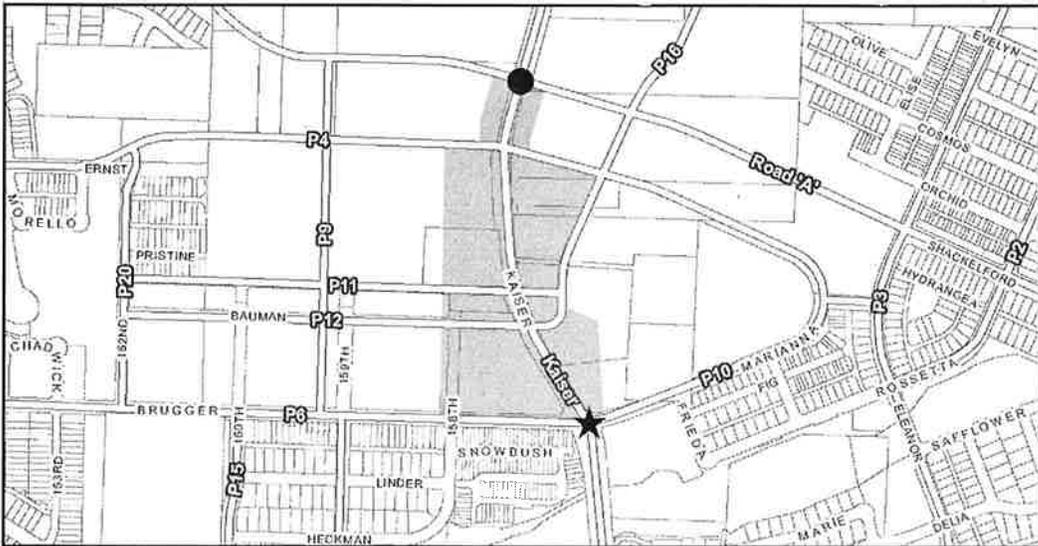
The 'Core Design Elements' map is amended as shown:

Current Configuration



- * Community Gateway to be Relocated
- Unchanged Community Gateway Location
- Primary Streets (Shown for Context)
- Metro 2040 Main Street Area Designation (Shown for Context)

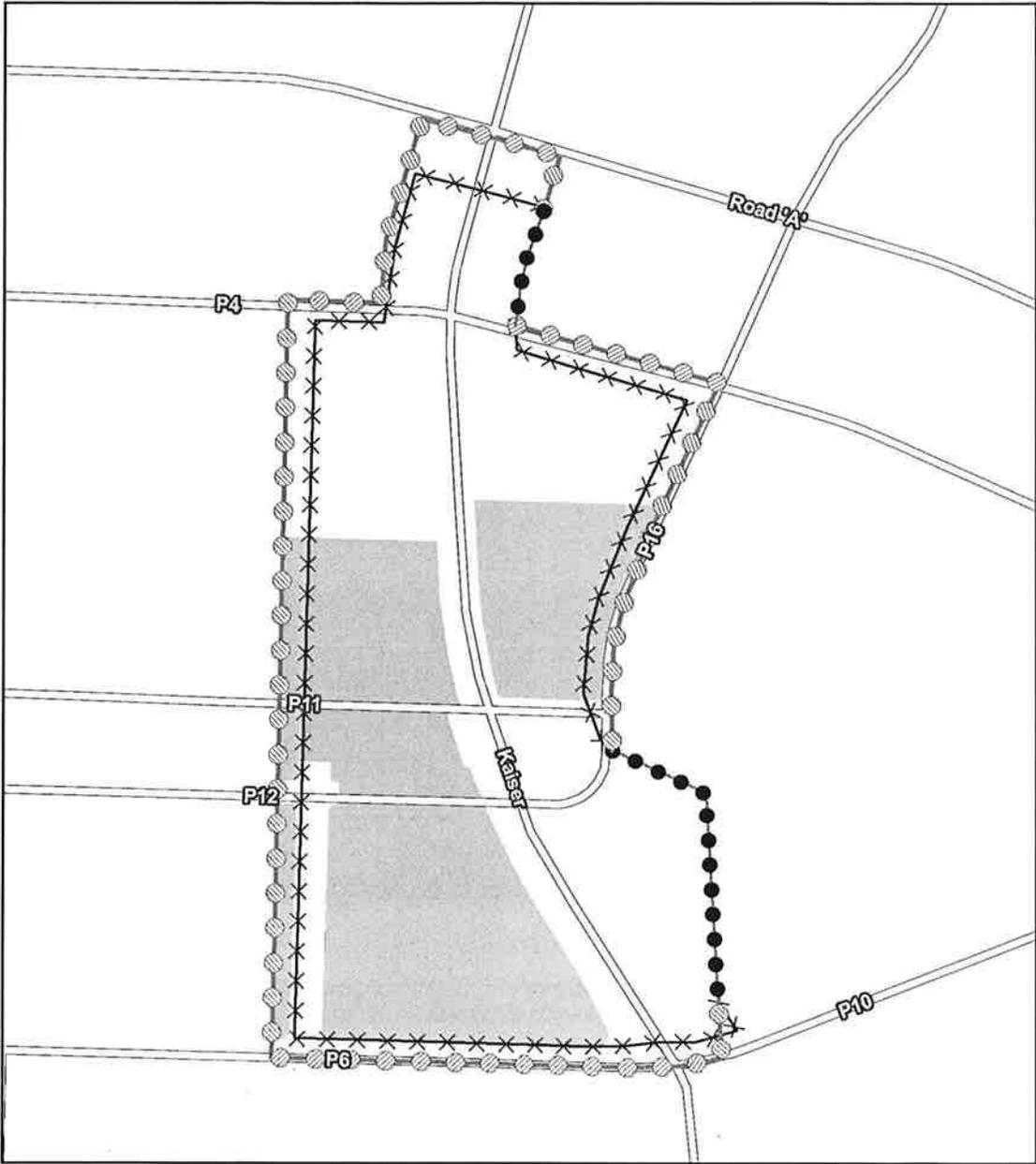
Proposed Configuration



- ★ Relocated Community Gateway Location
- Unchanged Community Gateway Location
- Primary Streets (Shown for Context)
- Metro 2040 Main Street Area Designation (Shown for Context)

abcdef Proposed additions
~~abcdef~~ Proposed deletions

The 'Areas of Special Concern' map is amended as shown:



- | | | | |
|---|--|---|--|
|  | Remove Boundary from Area of Special Concern Road Corridor 2 (ASC 2) |  | Primary Streets (Shown for Context) |
|  | New Boundary for ASC 2 |  | Neighborhood Commercial Mixed Use (NCMU NB) Boundaries (Shown for Context) |
|  | Unchanged Boundary of ASC 2 | | |

abcdef Proposed additions
 abcdef Proposed deletions

The North Bethany Main Street Program Guide of the BETHANY COMMUNITY PLAN is amended as shown:

Subsection E – North Bethany Main Street Program Guide

Introduction

This North Bethany Main Street Program Guide (Program Guide) ~~was written to identify~~ is plan and design requirements ~~that are specific to development within the designated 2040 Main Street boundary of the North Bethany Subarea. The Program Guide specifically applies to Area of Special Concern Road Corridor 2 identified in the North Bethany Subarea Plan. See Attachment 1 for a reference map.~~

The purpose of the Program Guide ~~is~~ was to provide guidance for more specific planning of the Main Street commercial area, which is a core design feature of the concept plan for the North Bethany Subarea. Before development of the Main Street Area ~~could~~ can begin, a specific Urban Design Plan for the area ~~was required to be developed. The Urban Design Plan was required to that addresses the criteria outlined in this Program Guide must be developed. The criteria~~ address important design and operational elements such as density, retail design and format, parking, ~~corner vision, sight distance, building entrances and orientations, placement of buildings close to sidewalks, placement of off-street vehicle parking generally behind building~~ façades and not adjacent to street frontages, access points and the need for turning movements and on-street parking along NW Kaiser Road. Therefore, prior to preliminary engineering of the Main Street section of NW Kaiser Road and prior to development of land located within this Area of Special Concern 2 ~~(including portions of lots or parcels),~~ a more detailed urban design plan ~~will~~ shall be prepared in accordance with the requirements of this Program Guide. ~~The preliminary commercial programming mix and size limits outlined in this Program Guide shall serve as a template for future specific design and development of the area. The commercial program in the Main Street Program Guide is based on the North Bethany Strategic Programming Guide (December 2006) prepared by Leland Consulting Group.~~

~~Prior to the adoption of the Urban Design Plan, the Board may permit development in Area of Special Concern 10 when development is consistent with the requirements of ASC 10 and other applicable requirements of the North Bethany Subarea Plan. ASC 10 was adopted to allow the owner of Tax Lot 800, Tax Map 1N1 17B, to develop Tax Lot 800 under a single development application beginning in 2012.~~

Background

The 2040 Concept for the Metro region identifies NW Kaiser Road as a “corridor.” Corridors are defined in the Urban Growth Management Functional Plan (UGMFP) as “along good quality transit lines, corridors feature a high-quality pedestrian environment, convenient access to transit, and somewhat higher than current densities.” Locating mixed-use commercial areas within easy walking distance of residences is a fundamental principle of “smart growth” planning because it provides a more “complete” community where daily needs and services can be met via multiple transportation modes and without the need for long trips. This concept of integrating residential uses with mixed/commercial use is consistent with the Main Street designation, and with Policy 13 (Urbanization), Policy 18 (Plan Designations and Locational Criteria for

abcdef Proposed additions

~~abcdef~~ Proposed deletions

Development), Policy 35 (Residential Conservation), Policy 36 (Commercial Conservation), Policy 39 (Land Use Conservation) and Policy 40 (Regional Planning Implementation) of the county's Comprehensive Framework Plan, as well as Policy 1 (Travel Needs), Policy 5 (System Implementation and Plan Management) and Policy 6 (Roadway System) of the County's Transportation System Plan.

Through the 2007 concept planning process for the North Bethany Subarea, the general scale, location and configuration of the commercial component was evaluated. ~~First, a market analysis was conducted to assess the broader market geography and context in order to determine a program (general size and mix) that would be viable given the planned residential capacity of the area at build-out. The report, prepared by Leland Consulting Group (LCG), prepared a report, the *North Bethany Strategic Programming Guide*, that recommended three possible configurations for consideration as part of the community design process. LCG continued to advise the process regarding various locations.~~

After much stakeholder and public input on the three potential configurations, the commercial/mixed-use components described in this program guide were selected along with a preferred format for a primary core commercial area complemented by a series of smaller "street corner" commercial nodes. This is reflected in the concept plan for the North Bethany Subarea, which refers to the primary commercial core as a Main Street area, and includes a series of individual commercial sites in the other neighborhoods within the Subarea. ~~The identified use categories for the Main Street area are general and will be determined through a review process at a time when the market dictates.~~

Urban Design Plan

When this Program Guide was originally adopted in 2010, it required an Urban Design Plan (UDP) to be adopted before applications for development of the Main Street area could be submitted. Development of the UDP through a public process began in 2017. The UDP was developed to promote a Main Street design that prioritizes pedestrian and bicycle environments and provides attractive and quality design for both the public realm (streets and sidewalks) and for private development.

The UDP recommendations were incorporated into Area of Special Concern (ASC) 2 of the North Bethany Subarea Plan as design standards for future development within the Main Street area. The UDP design standards incorporated into ASC 2 were adopted by ordinance in the fall of 2018.

~~Before an application for development of the Main Street area (or any part of the Main Street area) may be submitted, an Urban Design Plan for the Main Street Area shall be adopted. For the purposes of this Program Guide, an Urban Design Plan is defined as a planning effort focused on urban form that is intended to transform the vision for an area into reality. It is usually comprised of equal parts planning, urban design, investment strategy, development and design code, and action items. Since it deals with all the aspects of an area, from public improvements to private development, an urban design plan can bring together property owners, neighboring residents and public infrastructure providers while providing a better understanding and more predictable sense of how new development will be built.~~

Urban Design Plans are particularly useful for areas where local governments want to target investment, such as centers and corridors, because it brings all the aspects of planning, placemaking and infrastructure improvements together at the same time. Urban Design Plans can serve as the bridge between planning and its translation into a development proposal. Urban design plans can test existing or proposed zoning code provisions by illustrating their character and scale in order to ensure the zoning code text will support desired urban forms. Similarly, their strength at articulating a cohesive vision offers a more flexible implementation of objectives that still meet the vision. This is particularly important if unanticipated market trends occur.

Once developed, an urban design plan can be used to guide infrastructure investments and improvements by other property owners and different departments within the local government and service provider agencies. Involving interests such as THPRD, CWS and LUT's Engineering Division will help inform the design of areas within their control, making design and planning solutions more comprehensive and proposed improvements more likely to be implemented. Internal coordination through the Long Range Planning Section can link planning efforts with specific capital projects and funding sources.

Commercial and Development Program

The commercial and retail development program for North Bethany's Main Street area is based on the *North Bethany Strategic Programming Guide* prepared by Leland Consulting Group in December 2006. The program is summarized in a table, ~~provided as Attachment 2. The table provided below, as Attachment 2 that~~ is based on "Table 8: Combination Village Center-Neighborhood Node Concept" from the *North Bethany Strategic Programming Guide*. A development program, ~~by definition,~~ is both a narrative and numerical description of how a property or area ~~could~~ should be developed. The program draws upon the research conducted by the various professional disciplines in the consulting team, market conditions, stakeholder objectives, public policy, and other factors. The way the program is configured for the geography of the North Bethany Subarea responds to input provided by stakeholders and the public through the concept planning process. ~~Together, the North Bethany Subarea Plan and this Program Guide are guiding documents for the county and developers of the Main Street area that have responsibility for translating the program into a physical plan.~~

In preparing the *North Bethany Strategic Programming Guide*, the Leland Consulting Group did the following:

- ~~Examined the current market, trends, and needs;~~
- ~~Studied how the market interfaces with the County's vision for North Bethany and this project;~~
- ~~Recognized existing community assets, opportunities, and strengths;~~
- ~~Defined the program in terms of housing and commercial components; and~~
- ~~Developed program alternatives for the housing and commercial component.~~

The guiding language and information from this report was directly used in seeking physical solutions in the concept planning phase of the planning process. The *North Bethany Strategic Programming Guide* combines a blend of market research and market strategy appropriate for North Bethany with the recognition that a short-term strategy can overwhelm the long-term objectives unless communication and partnering is involved and respected.

~~The concept planning consultant team was charged with preparing a plan that can expect successful implementation while addressing the goals of the community or an agreed upon vision for the area. The additional and equally rigorous layer of considerations that impacted the program design consisted of:~~

- ~~▪ Market willingness to live, work and shop in the environment to be developed;~~
- ~~▪ Market capacity to pay for homes and rent office or retail space; and~~
- ~~▪ Achieving levels of profitability commensurate with risk.~~

~~Developing a commercial component supports the public objectives of reducing vehicle miles traveled (VMTs) in the area while providing convenient access to day-to-day shopping needs and services, as well as providing a gathering place for community building. Care was taken not to oversize the commercial component at the edge of the UGB next to the rural and forested area.~~

The market analysis conducted for this project recommended three variations on a feasible commercial program for North Bethany. The preferred program described in the table below~~Attachment 2~~—a mix of retail, office, residential and community uses—was selected by SWG members with public input from an open house design workshop in May 2007. The resulting program for the Main Street area₁—the core commercial node of the North Bethany Subarea₁—is designed to be complemented by a series of smaller neighborhood commercial nodes. Salient aspects & components of each are as follows:

Neighborhood Nodes

- for convenience
- located at visible corners
- small scale (can have other uses above)
- adjacent to other destinations (e.g., schools, parks, etc.)
- on-street parking is key
- examples include: coffee shop, café, juice bar, convenience store, nursery school

Commercial Core

- located in highly visible spot, along Kaiser Road arterial
- an anchor establishment with smaller support establishments
- recommended office (or other nonretail) uses in upper stories
- if residential included, it poses new parking issues that must be addressed
- integrated with primary community destinations, including park block elements and civic space/building

Process

~~The county shall prepare the Urban Design Plan in conjunction with the design of NW Kaiser Road, consistent with the requirements of ASC Road Corridor 2. A public planning process shall be used to develop and adopt this plan. Adoption of this plan and completion of the design for NW Kaiser Road shall be completed by the time there will be sufficient residential development in North Bethany to support the Main Street Area. Alternatively, a private applicant may initiate the development of the Urban Design Plan prior to the county doing so. Under this option, the applicant shall consult with LUT staff while developing the Urban Design Plan. Any proposed~~

~~Urban Design Plan shall be subject to a public process and there shall be coordination with affected property owners and service providers. The county will be responsible for initiating necessary legislative amendments to applicable elements of the Comprehensive Plan in order to implement the applicant's final Urban Design Plan.~~

~~Design Criteria~~

~~This section of the Program Guide elaborates on the design criteria that must be addressed in the required Urban Design Plan.~~

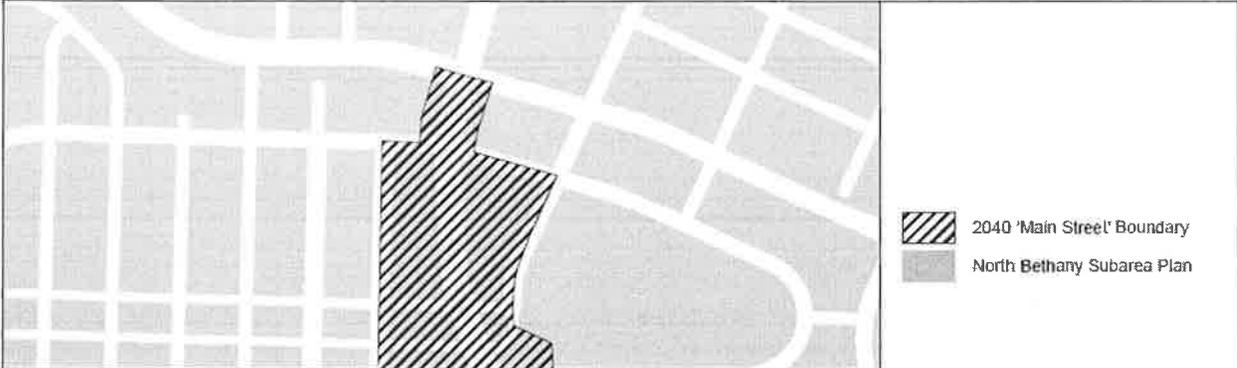
~~Consistent with Metro's 2040 concept, the design character of a Main Street area is distinct from a Town Center in terms of scale and format. The market capture area for a Main Street is generally an order of magnitude smaller than that of a Town Center. The square footage limits for the various types of commercial uses in North Bethany Subarea's Main Street that are stipulated in this Program Guide have been determined appropriate for the approximately 5,000 new dwelling units planned for the North Bethany and Arbor Oaks Subareas. Despite the implication of the term "main street," the programmed uses can be arranged either along a primary street or clustered around a prominent intersection, as is the case with the North Bethany Subarea Plan. While some parking is required for the programmed commercial and residential uses, it is intended that the format support the walkable character of the community. Buildings therefore are required to be close to sidewalks with parking generally located in the rear. The minimum setback requirements of the underlying land use district (Neighborhood Commercial Mixed Use) may be reduced to achieve this objective.~~

~~The Urban Design Plan shall show how parking will be formatted to accommodate the proposed residential density and the commercial program, including on-street and off-street parking. Parking lots shall be generally located behind retail façades and not adjacent to street frontages. The Urban Design Plan shall also address applicable requirements related to vehicular movements and operation, including access, sight distance, corner vision, design speed and turn movements. The Main Street area shall be designed to prioritize pedestrian and bicycle environments over motorized vehicles. This shall be reflected in all aspects of the design criteria. Accordingly, the Urban Design Plan shall also address building orientation and entrances.~~

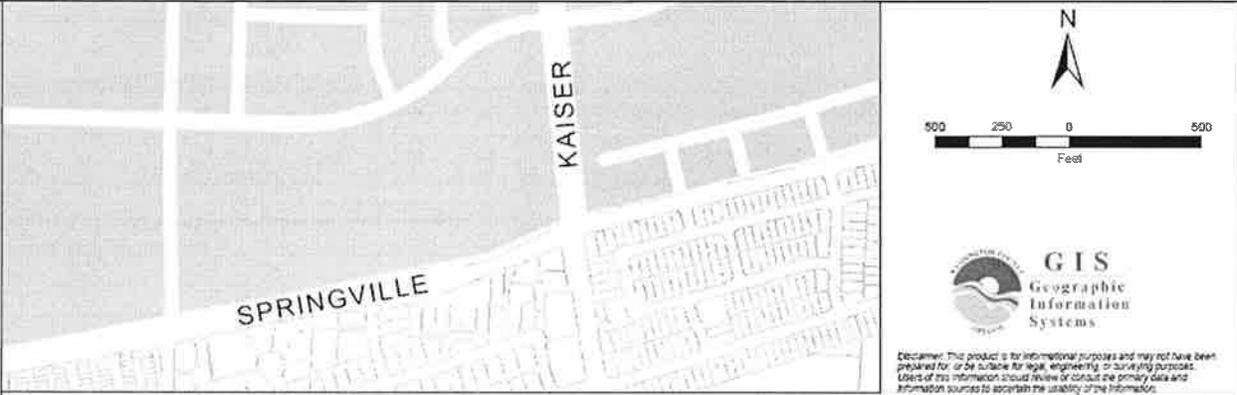
~~**Attachments 3 and 4** illustrate conceptual design solutions for the Main Street area and can serve as a starting place for discussion.~~

Attachment 1: Main Street Area

This map shows the boundaries of the Main Street area.



DELETED



Attachment 2: Development Program

The following table summarizes the proportion and mix of commercial and residential uses anticipated for the Main Street area of the North Bethany Subarea. ~~While~~ The estimated sizes for the various commercial and uses represent targets, ~~the estimated sizes for the various~~ residential uses are intended as guidelines only and may vary from what is indicated here. ~~It is required that the estimated sizes for the commercial uses achieve at least 85 percent of the estimated size in each category (i.e., Retail, Employment/Service, and Civic), and do not exceed the target by more than 5 percent.~~

DEVELOPMENT PROGRAM FOR MAIN STREET AREA*Source: *North Bethany Strategic Programming Guide*

Leland Consulting Group, December 2006

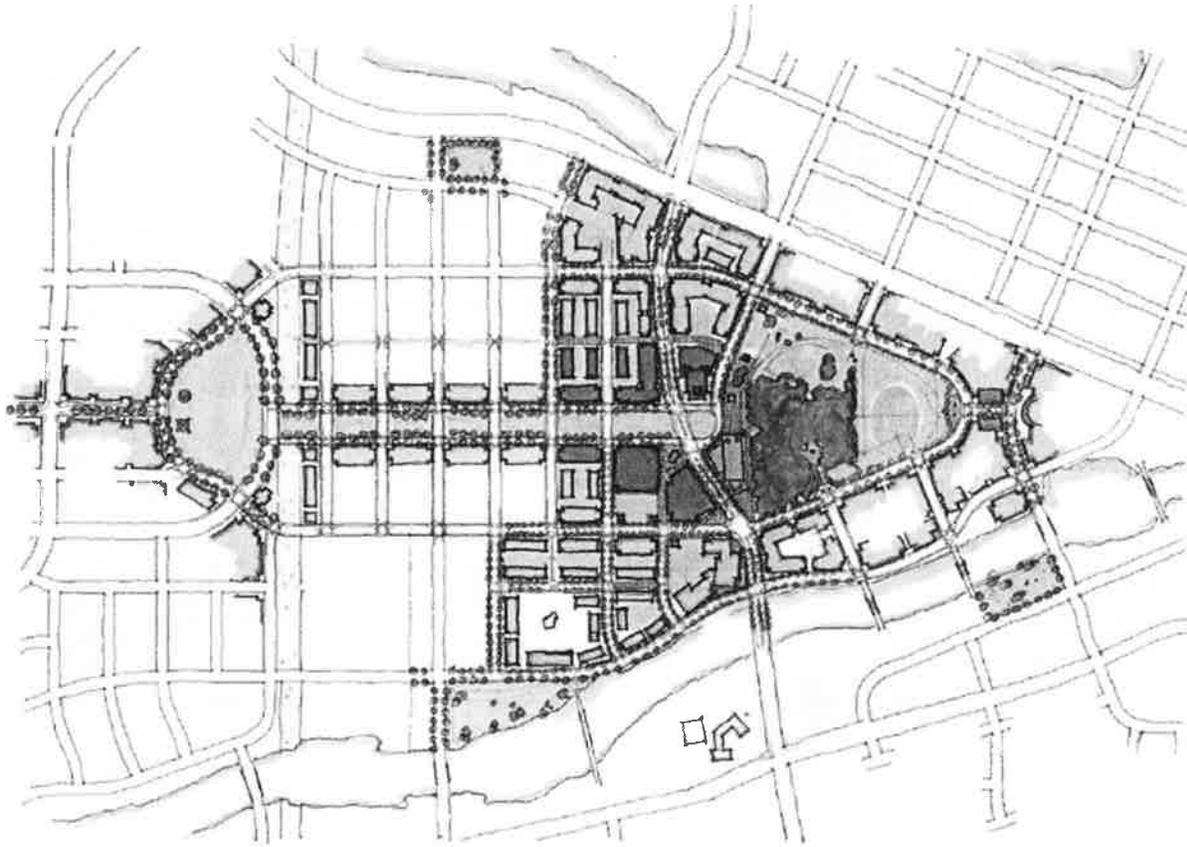
Land Use	Estimated Size (sf)
Retail Center	
Specialty Grocer	25,000
In-line Retail Shops	10,000
Restaurants	4,000
Retail Services	3,000
<i>Subtotal</i>	42,000
Employment / Service	
General Office	6,000
Fitness Center or other large services use	24,000
<i>Subtotal</i>	30,000
Civic Uses	
Branch Library	5,000
Community Center	3,500
<i>Subtotal</i>	8,500
Total: Nonresidential Uses	
	80,500
Housing	
Specialty Housing (150)	110,000
Apartments (125)	125,000
Condominiums (100)	185,000
Total: Residential Uses	
	420,000
total land area	approx. 7.5 acres

* Note this is complemented by neighborhood node development throughout the study area.

Combined total for node retail uses: 8,500 sf; for residential portion of mixed use node: 85,000 sf.

Attachment 3: Illustrative Concept Plan

This design sketch illustrates one way the preferred commercial program may be implemented.

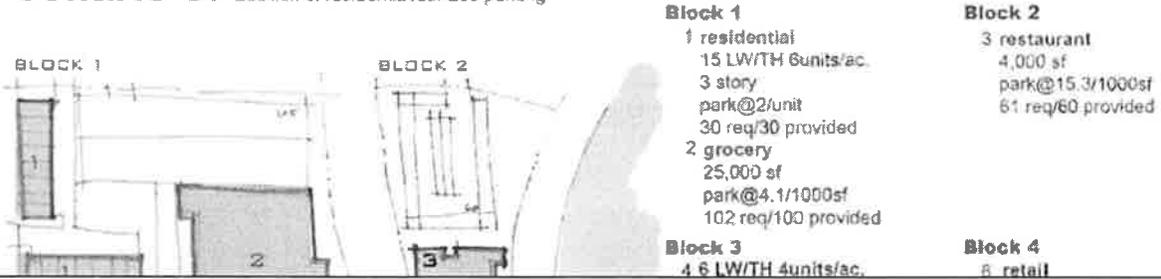


The core of the North Bethany Subarea is organized around two community park areas connected by a linear “park blocks” feature. High density housing is placed adjacent to park areas, with commercial “mixed-use” buildings (shown as red) clustered around NW Kaiser Road and located to frame street and pedestrian areas. A civic building, illustrated in purple, is prominently located on NW Kaiser Road. An adjacent community park is an opportunity for shared parking.

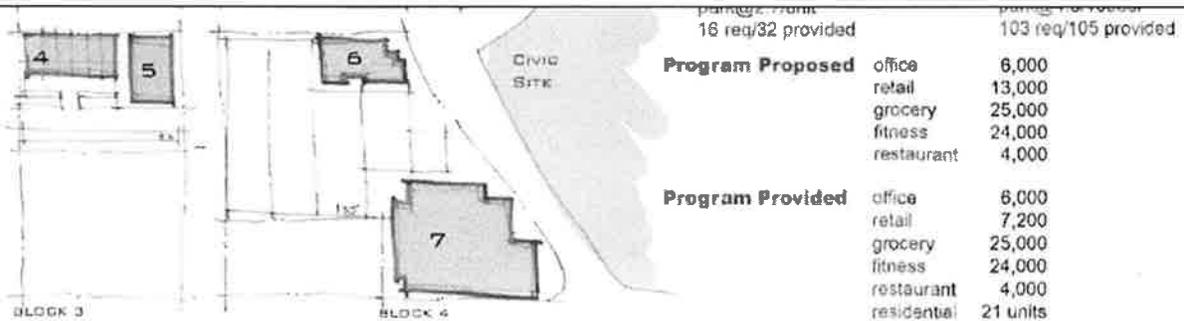
Attachment 4: Parking-Density Study

A series of design studies were conducted as part of the concept planning process for North Bethany to determine how the commercial program for the core commercial area could be arranged given various residential densities and parking formats (i.e., structured or surface parking). The following schematic illustrates the scenario that most closely fits the outcome of the planning process, namely with modest level of residential density and surface parking.

Senario 3: addition of residential/surface parking



DELETED



WITH A SMALL REDUCTION IN RETAIL SQUARE FOOTAGE, PARKING FOR THE FOUR BLOCKS CAN STILL BE SATISFIED WITH SURFACE PARKING. THE MASSING HOWEVER, WITH THE INTRODUCTION OF A THREE STORY RESIDENTIAL COMPONENT, NOW HAS NEWER URBAN FEELING, AND MOVES CLOSER TO TRUE MIXED-USE.

Section 390 (North Bethany Subarea Overlay District) of the COMMUNITY DEVELOPMENT CODE is amended to reflect the following:

390-14 Neighborhood Commercial Mixed-Use District (NCMU NB)

A. Lot Area: The minimum lot area shall be 8,500 square feet.

AGENDA

WASHINGTON COUNTY BOARD OF COMMISSIONERS

Agenda Category: Action – Land Use & Transportation (CPO 7)

Agenda Title: ADOPT FINDINGS FOR A-ENGROSSED ORDINANCE NO. 838

Presented by: Andrew Singelakis, Director of Land Use & Transportation

SUMMARY:

A-Engrossed Ordinance No. 838 amends the Comprehensive Framework Plan for the Urban Area, the Bethany Community Plan, and the Community Development Code to add requirements applicable to future development within the Main Street area of the North Bethany Subarea. A-Engrossed Ordinance No. 838 is posted on the County's land use ordinance webpage at the following link:

www.co.washington.or.us/landuseordinances

Post acknowledgment comprehensive plan amendments are amendments made to the County's Comprehensive Plan after it was acknowledged by the State Department of Land Conservation and Development as complying with the Statewide Planning Goals. ORS 197.615 requires such amendments be accompanied by findings setting forth the facts and analysis showing the amendments are consistent with the applicable Statewide Planning Goals, Oregon Revised Statutes, State Administrative Rules and the applicable provisions of Washington County's Comprehensive Plan.

Additionally, as required by Title 8 of Metro's Urban Growth Management Functional Plan (UGMFP), any amendment to a comprehensive plan or implementing ordinance shall be consistent with the requirements of the UGMFP.

Attached is the Resolution and Order to adopt the findings for A-Engrossed Ordinance No. 838. Prior to the Oct. 23, 2018 meeting, the proposed findings will be provided to the Board, posted on the above land use ordinance webpage and available at the Clerk's desk.

Attachment: Resolution and Order

Clerk's Desk Item: Ordinance Findings *(click to access electronic copy)*

DEPARTMENT'S REQUESTED ACTION:

Adopt the findings for A-Engrossed Ordinance No. 838 and authorize the Chair to sign the Resolution and Order memorializing the action.

COUNTY ADMINISTRATOR'S RECOMMENDATION:

I concur with the requested action.

RO 18-103

Agenda Item No.	<u>4.a.</u>
Date:	10/23/18

IN THE BOARD OF COMMISSIONERS
FOR WASHINGTON COUNTY, OREGON

In the Matter of Adopting) RESOLUTION AND ORDER
Legislative Findings in Support)
of A-Engrossed Ordinance No. 838) No. **18-103**

This matter having come before the Washington County Board of Commissioners (Board) at its meeting of October 23, 2018; and

It appearing to the Board that the findings contained in "Exhibit A" summarize relevant facts and rationales with regard to compliance with the Statewide Planning Goals, Oregon Revised Statutes and Administrative Rules, Washington County's Comprehensive Plan, and titles of Metro's Urban Growth Management Functional Plan relating to A-Engrossed Ordinance No. 838; and

It appearing to the Board that the findings attached and herein incorporated as "Exhibit A" constitute appropriate legislative findings with respect to the adopted ordinance; and

It appearing to the Board that the Planning Commission, at the conclusion of its public hearing on August 1, 2018, made a recommendation to the Board, which is in the record and has been reviewed by the Board; and

It appearing to the Board that, in the course of its deliberations, the Board has considered the record which consists of all notices, testimony, staff reports, and correspondence from interested parties, together with a record of the Planning Commission's proceedings, and other items submitted to the Planning Commission and Board regarding this ordinance; it is therefore,

RESOLVED AND ORDERED that the attached findings in "Exhibit A" in support of A-Engrossed Ordinance No. 838 are hereby adopted.

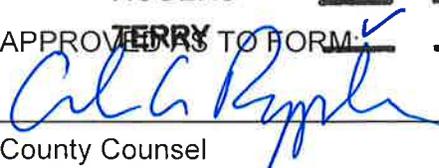
DATED this 23rd day of October, 2018.

	AYE	NAY	ABSENT
DUYCK	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SCHOUTEN	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
MALINOWSKI	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ROGERS	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

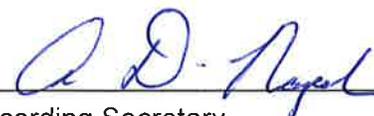
BOARD OF COMMISSIONERS
FOR WASHINGTON COUNTY, OREGON



Chairman

APPROVED TO FORM


County Counsel
For Washington County, Oregon



Recording Secretary

EXHIBIT A

FINDINGS FOR A-ENGROSSED ORDINANCE NO. 838

AN ORDINANCE AMENDING THE COMPREHENSIVE PLAN TO IMPLEMENT THE NORTH BETHANY MAIN STREET URBAN DESIGN PLAN

October 23, 2018

Part 1 – General Findings

Part 2 – Statewide Planning Goal Findings

Part 3 – Metro Urban Growth Management Functional Plan Findings

Part 4 – Metro Regional Transportation Functional Plan Findings

Part 1:

GENERAL FINDINGS

A-Engrossed Ordinance No. 838 amends the Comprehensive Framework Plan for the Urban Area (CFP), the North Bethany Subarea Plan (Plan) of the Bethany Community Plan, and the Community Development Code (CDC) to implement the North Bethany Main Street Urban Design Plan.

Key Ordinance Provisions

- Adds design standards for North Bethany’s Main Street area that will apply to future mixed-use, commercial and residential development in the area.
- Amends North Bethany’s Main Street Program Guide to remove duplicative and superseded elements and to change the commercial use size requirements to guidelines.
- Updates the street cross-section for the segment of NW Kaiser Road within the Main Street area.
- Removes the ‘Special Frontages’ designation from some streets within the Main Street area.
- Relocates a North Bethany community gateway from the intersection of NW Kaiser Road and Bethany Creek to the intersection of NW Kaiser Road and NW Brugger Road.
- Makes minor amendments to the boundary of North Bethany’s Main Street area to make it consistent with the boundary of the Metro 2040 Main Street area designation in the CFP.
- Adds dimensional requirements for the Neighborhood Commercial Mixed-Use (NCMU NB) District.
- Adds requirements for the maintenance of street furniture within the Main Street area.

Because the ordinance would make changes that do not affect compliance with Oregon’s Statewide Planning Goals (Goals), it is not necessary for these findings to address the Goals with respect to each amendment. The County Board of Commissioners (Board) finds that the Goals apply to amendments covered by these findings only to the extent noted in specific responses to individual applicable Goals, and that each amendment complies with the Goals. Goals 15 (Willamette River Greenway), 16 (Estuarine Resources), 17 (Coastal Shorelands), 18 (Beaches

and Dunes), and 19 (Ocean Resources) and related Oregon Administrative Rules (OAR) are not addressed because these resources are not located within Washington County.

The Board also finds that Goals 3 (Agricultural Lands), 4 (Forest Lands) and 14 (Urbanization) are not applicable because the area affected by this ordinance is entirely within the urban growth boundary.

The County is also required to make findings that the amendments are consistent with the requirements of Metro's Urban Growth Management Functional Plan (UGMFP) and Regional Transportation Functional Plan (RTFP). These findings are addressed in this document.

Part 2:
STATEWIDE PLANNING GOAL FINDINGS

The purpose of the findings in this document is to demonstrate that A-Engrossed Ordinance No. 838 is consistent with Statewide Planning Goals (Goals), Oregon Revised Statutes (ORS), Oregon Administrative Rule (OAR) requirements, Metro's Urban Growth Management Functional Plan (UGMFP), and Washington County's Comprehensive Plan (Plan). The County's Plan was adopted to implement the aforementioned planning documents and was acknowledged by the State of Oregon. The County follows the post-acknowledgement plan amendment (PAPA) process to update the Plan with new state and regional regulations as necessary and relies in part upon these prior state review processes to demonstrate compliance with all necessary requirements. No goal compliance issues were raised in the hearing proceedings described below. In addition, none of the proposed changes to the map and text of the Plan implicate a goal compliance issue. The following precautionary findings are provided to demonstrate ongoing compliance.

Goal 1 - Citizen Involvement

Goal 1 addresses Citizen Involvement by requiring the implementation of a comprehensive program to stimulate citizen participation in the planning process. Washington County has an acknowledged citizen involvement program that provides a range of opportunities for citizens and other interested parties to participate in all phases of the planning process. In addition, Chapter X of the County's Charter sets forth specific requirements for citizen involvement during review and adoption of land use ordinances. Washington County has followed these requirements for the adoption of A-Engrossed Ordinance No. 838.

Goal 2 - Land Use Planning

Goal 2 addresses Land Use Planning by requiring an adequate factual base to support a decision as well as coordination with affected governmental entities. Washington County has an acknowledged land use planning process that provides for the review and update of the various elements of the Plan, which includes documents such as the Rural/Natural Resource Plan, Comprehensive Framework Plan for the Urban Area (CFP), Community Plans, Community Development Code (CDC), and Transportation System Plan (TSP). Washington County utilized this process to adopt A-Engrossed Ordinance No. 838.

Notice was coordinated with all affected governmental entities and comments received regarding A-Engrossed Ordinance No. 838 were addressed either as part of the proceedings or with subsequent staff coordination.

Goal 5 – Natural Resources, Scenic and Historic Areas, and Open Spaces

Goal 5 addresses the protection of natural resources and the conservation of scenic, cultural, and historic areas and open spaces by requiring local programs to protect these resources in order to promote a healthy environment and natural landscape that contributes to Oregon’s livability for present and future generations.

In addition, OAR 660-023-0250 requires application of current Goal 5 provisions to post-acknowledgment plan amendments (PAPAs) when the PAPA 1) creates or amends a resource list or a portion of an acknowledged plan or land use regulation that protects a significant Goal 5 resource, or 2) allows new uses that could be conflicting uses with a particular Goal 5 site.

Policies 10, 11 and 12 of the CFP, Policies 7, 9, 10, 11, 12 and 13 of the Rural/Natural Resource Plan, and various sections of the Community Plans and the CDC include provisions for the protection of Goal 5 resources.

A-Engrossed Ordinance No. 838 does not amend any of the Goal 5 significant natural resource designations previously adopted as part of the North Bethany Subarea of the Bethany Community Plan. The amendments made by A-Engrossed Ordinance No. 838 are consistent with the County’s acknowledged policies and standards for the protection of Goal 5 resources, as well as those set forth in OAR 660 Division 23.

Goal 6 - Air, Water and Land Resources Quality

Goal 6 requires the maintenance and improvement of the quality of the air, water and land resources of the state through the implementation of local plans that address waste and process discharge. Policies 4, 5, 6 and 7 of the CFP and Policies 4, 5, 6, and 7 of the Rural/Natural Resource Plan provide for the maintenance and improvement of the quality of air, water and land resources.

A-Engrossed Ordinance No. 838 does not amend the Plan policies or CDC standards related to air, water or land resources that impact the County’s compliance with Goal 6. A-Engrossed Ordinance No. 838 does not amend any provisions regarding Community Plan and CDC protections to significant wetlands, air quality or land resource quality. Plan compliance with Goal 6 is maintained with the amendments made by A-Engrossed Ordinance No. 838. The amendments are consistent with the County’s acknowledged policies and standards for protection of Goal 6 resources.

Goal 7 - Areas Subject to Natural Hazards

Goal 7 requires the implementation of local land use programs that reduce the risk to people and property from natural hazards such as floods, landslides and earthquakes. Policy 8 of the CFP and Policy 8 of the Rural/Natural Resource Plan set out the County’s policy to protect life and property from natural disasters and hazards.

A-Engrossed Ordinance No. 838 does not amend the applicable Plan policies and strategies or CDC sections related to flood plain areas, or to natural disasters and hazards. Plan compliance with Goal 7 is maintained with the amendments made by A-Engrossed Ordinance No. 838. The amendments are consistent with the County’s acknowledged policies and standards for regulating development exposed to potential natural disasters and hazards addressed by Goal 7.

Goal 8 - Recreational Needs

Goal 8 requires local jurisdictions to satisfy the recreational needs of citizens and visitors by planning and providing for the siting of necessary recreational facilities. Policies 33, 34 and 35 of the CFP, Policy 24 of the Rural/Natural Resource Plan and the individual Community Plans address the recreational needs of Washington County’s residents and visitors.

The North Bethany Subarea incorporates a plan for various park locations and sizes, along with a network of multimodal off-street recreational trails and on-street connections. This program of land for recreational facilities is consistent with the level of service standards for the identified park service provider, as described in the Master Plan of the Tualatin Hills Park and Recreation District (THPRD), and was adopted in 2010 via A-Engrossed Ordinance No. 730.

A-Engrossed Ordinance No. 838 does not amend North Bethany’s park locations or configurations, or the location and alignment of future planned trails or on-street connections. Plan compliance with Goal 8 is maintained with the amendments made by A-Engrossed Ordinance No. 838. The amendments are consistent with the County’s acknowledged policies and strategies for satisfying recreational needs as required by Goal 8.

Goal 9 – Economic Development

Goal 9 requires the provision of adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare and prosperity of citizens. Policy 20 of the CFP and Policies 15, 16, 20 and 21 of the Rural/Natural Resource Plan set out the County’s policies to strengthen the local economy. The CDC contributes to a sound economy by providing standards that facilitate development in an orderly and efficient fashion.

A-Engrossed Ordinance No. 838 does not amend any County policies or CDC requirements regarding the local economy. However, the adoption of Main Street design standards through A-Engrossed Ordinance No. 838 will allow applications for development in the Main Street area to be submitted. The Main Street area contains North Bethany’s largest concentration of properties with a commercial land use designation, and that designation, Neighborhood Commercial Mixed Use, allows for North Bethany’s greatest range of commercial land uses. Therefore, A-Engrossed Ordinance No. 838 is likely to spur economic development of North Bethany’s Main Street area. Plan compliance with Goal 9 is maintained with the amendments made by A-Engrossed Ordinance No. 838.

Goal 10 - Housing

Goal 10 requires the provision of housing, including adequate numbers of units within a range of prices, types and densities that provide realistic options to meet citizen needs. Policies 21, 22, 23 and 24 of the CFP, and Policies 19 and 25 of the Rural/Natural Resource Plan address the provision of housing in the urban and rural areas of the county. The CDC contributes to the provision of adequate housing by establishing standards that facilitate development in an orderly and efficient fashion.

In 2010, the North Bethany Subarea Plan (Plan) was adopted by A-Engrossed Ordinance No. 730, and included a variety of residential land use designations that allow units in a range of housing types and densities. A-Engrossed Ordinance No. 730 was found to comply with Goal 10.

A-Engrossed Ordinance No. 838 does not change any adopted North Bethany land use designations or amend any County policies regarding the provision of housing. Plan compliance with Goal 10 is maintained with the amendments made by A-Engrossed Ordinance No. 838.

Goal 11 - Public Facilities and Services

Goal 11 requires a plan for the orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural development. Policies 15, 25, 26, 27, 28, 29, 30 and 31 of the CFP and Policy 22 of the Rural/Natural Resource Plan address the provision of public facilities and services in the urban and rural areas of unincorporated Washington County. The CDC requires that adequate public facilities and services be available for new development.

A-Engrossed Ordinance No. 838 is consistent with the County's acknowledged policies and strategies for the provision of public facilities and services as required by Goal 11. Plan compliance with Goal 11 is maintained with the amendments made by A-Engrossed Ordinance No. 838.

Goal 12 - Transportation

Goal 12 requires the provision and encouragement of a safe, convenient, multimodal, and economic transportation system. Policy 32 of the CFP, Policy 23 of the Rural/Natural Resource Plan, and in particular the Washington County Transportation System Plan (TSP) describe the transportation system necessary to accommodate the transportation needs of Washington County. Implementing measures are contained in the TSP, Community Plans, and the CDC.

A-Engrossed Ordinance No. 838 amends the CFP, Bethany Community Plan, and CDC to implement the North Bethany Main Street Urban Design Plan to add design and dimensional requirements applicable to future development within the Main Street area of the North Bethany Subarea. The only amendments that relate directly to transportation are updates to the street cross-section for the segment of NW Kaiser Road within the Main Street area, and the requirement for parking lots to include separated pedestrian facilities that connect to the sidewalk and building entrances.

A-Engrossed Ordinance No. 838 updates the cross-section of NW Kaiser Road within the Main Street area providing additional separation distance using painted buffers between the vehicle parking lane, the bicycle lane, and the vehicle travel lane, and increases the segment's overall right-of-way width from approximately 98 feet to approximately 102 feet. Based on design guidance provided in the Washington County Bicycle Facility Design Toolkit, these changes are intended to encourage greater rates of bicycling and walking by lowering motor vehicle speeds and reducing the risk of dooring incidents, which can improve safety and increase comfort for roadway users. There are no significant changes to the proposed roadway capacity of NW Kaiser Road.

A-Engrossed Ordinance No. 838 also requires surface parking lots along non-designated streets within the Main Street area to have safe and comfortable pedestrian connections to building entrances. The pedestrian connections are to be: 1) physically separated from adjacent vehicle parking and from parallel vehicle traffic through the use of curbs and landscaping, 2) are at minimum 5 feet wide, and 3) are paved or striped to distinguish them from parking or vehicle circulation areas. The ordinance also stipulates that landscaping must be used around parking and vehicle circulation areas adjacent to the street right-of-way or pedestrian accessways. The parking lot requirements are consistent with the parking standards of the CDC adopted by A-Engrossed Ordinance No. 827 and therefore are consistent with the Transportation Planning Rule (TPR).

A-Engrossed Ordinance No. 838 does not amend the TSP, nor does it include any transportation-related amendments to the CDC. Since A-Engrossed Ordinance No. 838 does not amend the TSP or other policies related to transportation planning, only section 660-012-0060 of the TPR is applicable.

660-012-0060 This section sets forth requirements for plan and land use regulation amendments.

FINDING: A-Engrossed Ordinance No. 838, together with previously adopted and acknowledged ordinances, fully implements all of the applicable provisions of OAR 660-012-0060 as detailed in the following findings of fact:

- A-Engrossed Ordinance No. 838 does not change allowed land uses, zoning maps, density or type of development allowed to an extent that would add trips to the transportation system or change the existing or anticipated level-of-service or the level-of-service standard for any facility.
- A-Engrossed Ordinance No. 838 does not modify the functional classification or change the type or level of travel or access inconsistent with the designation of any existing or planned transportation facility.
- A-Engrossed Ordinance No. 838 amends the design standards for NW Kaiser Road within the Main Street area.
 - The amendment to the design standards is consistent with the planned function, capacity and performance standard of the roadway identified in the North Bethany Subarea Plan and the TSP.
 - The amendment to the design standards continues to provide an adequate multimodal facility to serve the proposed land uses in the Main Street and surrounding areas.

- A funding plan for Kaiser Road has been identified and adopted as a component of the North Bethany Funding Strategy adopted in 2011 by R&O 11-70.
- The design standards included in A-Engrossed Ordinance No. 838 are intended to improve the pedestrian and bicycle environment on Kaiser Road consistent with the policies of the TPR, while maintaining the motor-vehicle performance consistent with the Interim Washington County Motor Vehicle Performance Measures adopted by A-Engrossed Ordinance No. 768.
- While A-Engrossed Ordinance No. 838 does affect the planned design of Kaiser Road, the amendments are consistent with measures described in 660-012-0060(2) and therefore are consistent with 660-012-0060.

The amendments are consistent with the County’s acknowledged policies and strategies for the provision of transportation facilities and services as required by Goal 12 the Transportation Planning Rule (TPR), implemented via OAR Chapter 660, Division 12. Plan compliance with Goal 12 is maintained with the amendments made by A-Engrossed Ordinance No. 838.

Goal 13 - Energy Conservation

Goal 13 requires developed land uses to be managed and controlled so as to maximize the conservation of all forms of energy, based upon sound economic principles. Policies 36, 37, 38, 39 and 40 of the CFP, and Policy 25 of the Rural/Natural Resource Plan address energy conservation in the urban and rural areas of unincorporated Washington County. The CDC implements the energy conservation policies by establishing standards that promote energy efficient development, especially in Article IV.

A-Engrossed Ordinance No. 838 does not amend the applicable Plan policies and strategies or CDC sections related to energy conservation, therefore compliance with Goal 13 is maintained with the amendments made by Ordinance No. 838. The amendments are consistent with the County’s acknowledged policies and strategies for promoting energy conservation as required by Goal 13.

Part 4:

URBAN GROWTH MANAGEMENT FUNCTIONAL PLAN FINDINGS

Section 3.07.810 of Title 8 of Metro’s Urban Growth Management Functional Plan (UGMFP) requires that county comprehensive plan changes be consistent with the UGMFP. The following A-Engrossed Ordinance No. 838 findings have been prepared to address Title(s) 1, 3, 4, 6, 7, 8, 11, 12, 13 and 14 of the UGMFP.

Title 1 - Housing Capacity

Title 1 requires a city or county to maintain or increase its housing capacity (except as provided in Section 3.07.120) per the Regional Framework Plan which calls for a compact urban form and a “fair share” approach to meeting housing needs.

A-Engrossed Ordinance No. 838 does not amend County policies related to housing capacity. A-Engrossed Ordinance No. 838 is consistent with Title 1.

Title 3 - Water Quality and Flood Management

Title 3 protects beneficial water uses and functions and values of resources within Water Quality and Flood Management Areas by limiting or mitigating impacts from development activities and protecting life and property from dangers associated with flooding.

A-Engrossed Ordinance No. 838 does not amend any Plan policies or CDC standards related to water quality or flood management. A-Engrossed Ordinance No. 838 is consistent with Title 3.

Title 4 – Industrial and Other Employment Areas

Title 4 seeks to provide and protect a supply of sites for employment by limiting the types and scale of non-industrial uses in Regionally Significant Industrial Areas (RSIAs), Industrial and Employment Areas. Title 4 also seeks to provide the benefits of “clustering” to those industries that operate more productively and efficiently in proximity to one another than in dispersed locations. Title 4 further seeks to protect the capacity and efficiency of the region’s transportation system for the movement of goods and services and to encourage the location of other types of employment in Centers, Corridors, Main Streets and Station Communities.

The amendments in A-Engrossed Ordinance No. 838 do not affect protection of RSIAs, Industrial and Employment Areas, or the location of employment areas in Metro-designated Centers, Corridors, Main Streets and Station Communities. A-Engrossed Ordinance No. 838 does not affect compliance with Title 4.

Title 6 - Centers, Corridors, Station Communities and Main Streets

Title 6 calls for enhancements of Centers, Corridors, Station Communities and Main Streets as principal centers of urban life in the region via actions and investments by cities and counties, complemented by regional investments.

A-Engrossed Ordinance No. 838 will allow development to occur within North Bethany’s Main Street area, thereby helping to implement Metro 2040 Main Street goals. A-Engrossed Ordinance No. 838 is consistent with Title 6.

Title 7 - Housing Choice

To increase the supply of affordable housing opportunities, Title 7 implements policies of the Regional Framework Plan regarding establishment of voluntary affordable housing production goals to be adopted by local governments.

A-Engrossed Ordinance No. 838 does not amend County policies regarding affordable housing production goals. A-Engrossed Ordinance No. 838 is consistent with Title 7.

Title 8 - Compliance Procedures

Title 8 sets forth Metro's procedures for determining compliance with the UGMFP. Included in this title are steps local jurisdictions must take to ensure that Metro has the opportunity to review amendments to comprehensive plans. Title 8 requires jurisdictions to submit notice to Metro at least 35 days prior to the first evidentiary hearing for a proposed amendment to a comprehensive plan.

Consistent with Title 8, a copy of proposed Ordinance No. 838 was sent June 27, 2018 to Metro, 35 days prior to the first evidentiary hearing. A copy of A-Engrossed Ordinance No. 838 was sent Sept. 21, 2018 to Metro. Metro has not commented on A-Engrossed Ordinance No. 838.

Title 11 - Planning For New Urban Areas

Title 11 guides planning of urban reserves and areas added to the urban growth boundary for conversion from rural to urban use. Title 11 includes requirements that the development of areas added to the urban growth boundary implement the Regional Framework Plan and the 2040 Growth Concept.

A-Engrossed Ordinance No. 838 applies to lands within the urban growth boundary that are already designated for urban use. A-Engrossed Ordinance No. 838 does not affect compliance with Title 11.

Title 12 – Protection of Residential Neighborhoods

Title 12 protects existing residential neighborhoods from air and water pollution, noise and crime, and provides adequate levels of public services.

A-Engrossed Ordinance No. 838 does not impact compliance with Plan policies or CDC standards related to air or water pollution, noise or crime, or adequate levels of public services.

Title 13 – Nature in Neighborhoods

Title 13 conserves, protects and restores a continuous ecologically viable streamside corridor system integrated with upland wildlife habitat and the urban landscape.

A-Engrossed Ordinance No. 838 does not impact Plan policies or CDC standards related to streamside corridors or upland wildlife habitat.

Title 14 – Urban Growth Boundary

Title 14 prescribes criteria and procedures for amendments to the urban growth boundary to provide a clear transition from rural to urban development, an adequate supply of urban land to accommodate long-term population and employment, and a compact urban form.

A-Engrossed Ordinance No. 838 does not propose to amend the urban growth boundary. Compliance with Title 14 is not affected by A-Engrossed Ordinance No. 838.

Part 4:
REGIONAL TRANSPORTATION FUNCTIONAL PLAN FINDINGS

This section addresses the consistency of A-Engrossed Ordinance No 838 with the applicable policies of Metro’s Regional Transportation Functional Plan (RTFP). The Board finds that the RTFP applies to the amendments covered by these findings only to the extent noted in specific responses to the applicable elements of this plans, as provided below, and that the amendments comply with the applicable goals and policies of the RTFP.

Title 1 - Transportation System Design

Title 1 pertains to street construction and reconstruction. Section 3.08.110A requires designs that improve safety, support adjacent land uses and balance the needs of all users.

A-Engrossed Ordinance No. 838 requires NW Kaiser Road to be constructed as a complete street consistent with regional street design policies and providing low-impact development approach (LIDA) treatments for storm water. A-Engrossed Ordinance No. 838 continues to require roadways and other transportation facilities within the Main Street area, to be consistent with the Washington County Road Design & Construction Standards and Community Development Code. Taken together with A-Engrossed Ordinance No. 768 and A-Engrossed Ordinance No. 783, the North Bethany area is planned to provide for a transportation system design consistent with all the requirements of Title 1. The changes adopted by A-Engrossed Ordinance No. 838 continue or expand upon system design requirements consistent with Title 1.

Title 2 - Development and Update of Transportation System Plans

Title 2 identifies the process for developing a TSP within the Metro region. Provisions include identification of transportation system performance, needs, and solutions.

A-Engrossed Ordinance No. 838 does not change the Washington County TSP system performance, needs, or solutions.

Title 3 - Transportation Project Development

Title 3 pertains to the general location and size of transportation facilities.

A-Engrossed Ordinance No. 838 does not change the planned general location of any transportation facilities. A-Engrossed Ordinance No. 838 does modify the size and design to improve the pedestrian and bicycle environment on Kaiser Road consistent with the policies of the RTFP.

Title 4 – Regional Parking Management

Title 4 pertains to parking management and standards. Section 3.08.410F requires cities and counties to provide street-like features, including curbs, sidewalks and street trees or planting strips for parking lots larger than three acres.

A-Engrossed Ordinance No. 838 requires surface parking lots along non-designated street frontages within the Main Street area to have safe and comfortable pedestrian connections to building entrances that are 1) physically separated from adjacent vehicle parking and from parallel vehicle traffic through the use of curbs and landscaping, 2) are at minimum 5 feet wide, and 3) are paved or striped to distinguish them from parking or vehicle circulation areas. The ordinance also includes language stating that landscaping must be used around parking and vehicle circulation areas adjacent to the street right-of-way or pedestrian accessways.

Title 5 - Amendment of Comprehensive Plans

This section pertains to amendment of the Comprehensive Plan and the TSP.

A-Engrossed Ordinance No. 838 was developed consistent with the policy framework identified in the TSP and the projects identified are consistent with the projects identified in the 2014 RTP. As described previously in these findings, this process is consistent with all of the requirements of the RTFP.

Title 6 - Compliance Procedures

This section pertains to requirements associated with amendments to the Washington County TSP.

The adoption of A-Engrossed Ordinance No. 783 and its associated Technical Appendices compiled with the RTFP requirement for an update of the Washington County TSP by the end of 2014. Ordinance No. 838 makes no amendments to the TSP that would be inconsistent with the RTFP.